



Planning Commission Study Session

TO: PLANNING COMMISSION

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MEETING DATE: AUGUST 1, 2018

SUBJECT: Z18-08: GATEWAY AREA STANDARDS AND STREETScape GUIDELINES: INITIATION OF AN AMENDMENT TO THE TOWN OF GILBERT LAND DEVELOPMENT CODE, CHAPTER II: DESIGN STANDARDS AND GUIDELINES, ARTICLE 1.6: GATEWAY AREA RIGHT-OF-WAY IMPROVEMENTS STANDARDS AND STREETScape DESIGN GUIDELINES.

STRATEGIC INITIATIVE: Community Livability

The proposed amendment encourages a continued high level of development for the Gateway Area and promotes safety.

RECOMMENDED MOTION

- A. Staff recommends that the Planning Commission initiate a text amendment to the Land Development Code of Gilbert, Arizona Chapter II: Design Standards and Guidelines, Article 1.6: Gateway Area Right-of-Way Improvements Standards and Streetscape Design Guidelines.
- B. Conduct Citizen Review meeting to discuss the proposed LDC text amendment.

BACKGROUND/DISCUSSION

The Gateway Area Right-of-Way (ROW) Improvement Standards and Streetscape Design Guidelines were adopted by the Town Council on July 22, 2003 under Ordinance No. 1499 and amended on September 23, 2003. The document is now 15 years old and is in need of an update to comply with safety needs, existing improvements and current standards. These standards and guidelines are applied to streets within the Gateway Character Area. The area is bound by Warner Road to Pecos Road and Santan Freeway/Union Pacific Railroad/Higley Road to Power Road, an area map has been provided.

The effect of the amendment will be to update and clarify - development standards and guidelines applicable to street rights-of-way, intersections, landscape buffer easements, and streetscape elements within the Gateway Area. The goal is to resolve discrepancies and update the document with recent engineering standards and maintenance requirements. The amendment will assess the entire document to determine what should be retained, deleted, and/or amended. Below is a breakdown of each section with staff's initial analysis.

1.0 Introduction and Area Map

The introduction will be revised and an updated, clearer, Gateway Area map will be provided.

2.0 Street Rights-of-Way

Some of the cross sections for the arterials, collectors and local streets do not match what was constructed due to: a) changes in the field during capital improvement projects and b) rezoning's with planned area developments that approved revised cross-sections. The amendment will update the cross sections to reflect as-builts and future improvements.

3.0 Intersections

The intersection figures will be updated to reflect as-builts for the associated intersections, ADA requirements, and revised engineering standards. The Village Center boundary will be modified and the Concept Plan will be deleted.

4.0 Landscape Buffer Easements

The Landscape Buffer Easements may be modified due to in-the-field changes and utility company requirements.

5.0 Streetscape Elements

The streetscape elements will be assessed and revised to meet current product availability, allow for design options, new safety requirements and maintenance needs. Some examples of these changes are listed, but not limited to: a) fluted light pole requirements will be updated due to maintenance, rusting issues, and safety concerns; b) sissou trees have been reported as a nuisance for maintenance due to invasive roots; c) pruning figure exhibits will be removed as they are not needed in a design guidelines and standards document; and d) bus shelters details will be removed.

Staff is still in the beginning stages of amending the ROW standards and streetscape design guidelines. Staff is requesting the Planning Commission initiate the text amendment and provide feedback. It is the Town of Gilbert's goal to remain one of the most desirable Arizona

communities to live in and the Gateway Character Area is a major area for future town development. This amendment will refresh the design standards, ensuring that maintenance and safety are addressed for the longevity and enduring quality of the area.

STAFF REQUEST

- A. Staff recommends that the Planning Commission initiate a text amendment to the Land Development Code of Gilbert, Arizona Chapter II: Design Standards and Guidelines, Article 1.6: Gateway Area Right-of-Way Improvements Standards and Streetscape Design Guidelines.
- B. Conduct Citizen Review meeting to discuss the proposed LDC text amendment.

Respectfully submitted,

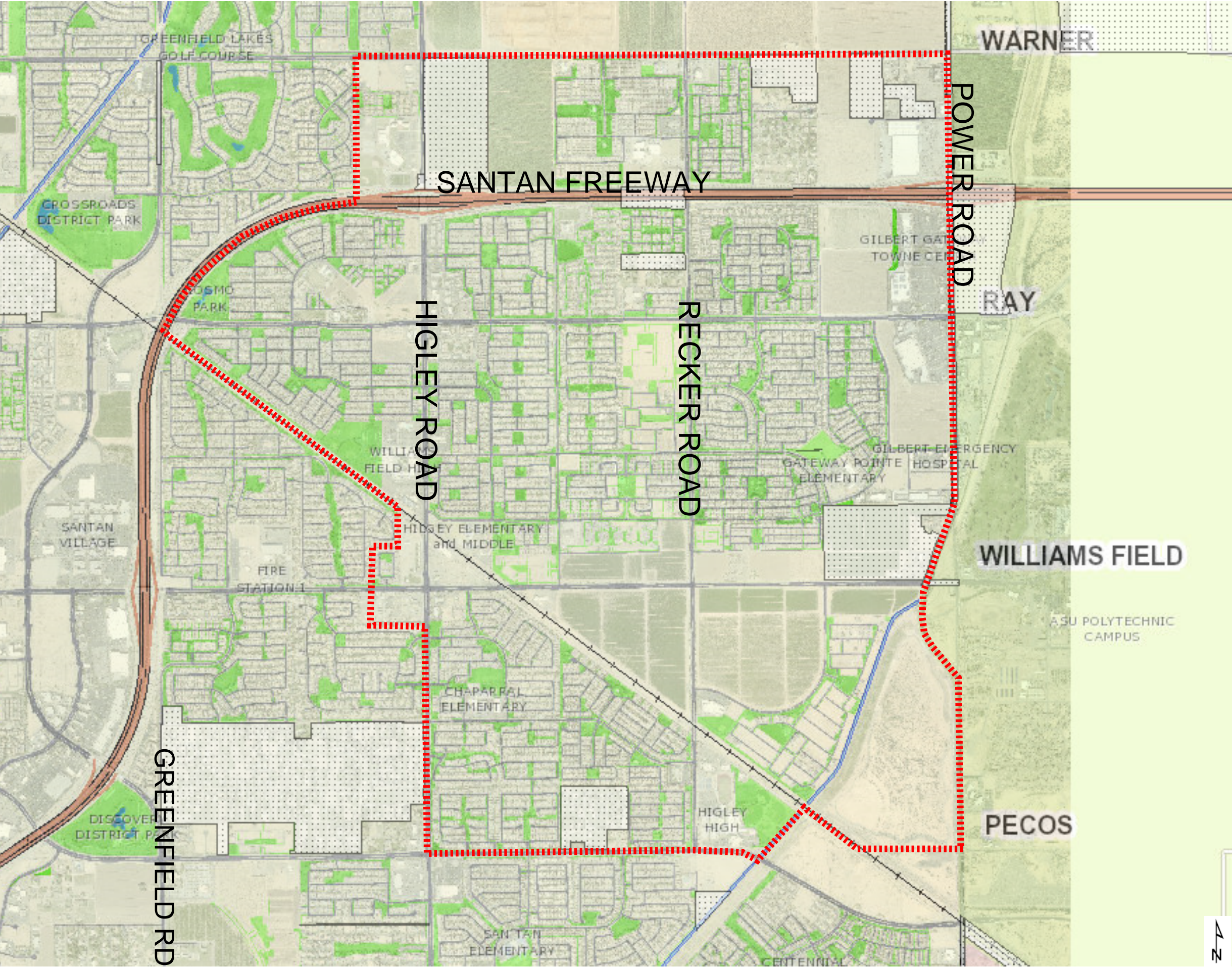


Stephanie Bubenheim,
Planner II

Attachments and Enclosures:

- 1) Area Map
- 2) Gateway Area Right-of-Way Improvement Standards and Streetscape Design Guidelines

GATEWAY CHARACTER AREA



Town of Gilbert Gateway Area Right-of-Way Improvement Standards and Streetscape Design Guidelines



DR03-47 Ord 1499 7.22.03

ADOPTED
July 18, 2003
AMENDED SEPTEMBER 23, 2003

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The Town of Gilbert would like to thank members of the Gateway Steering Committee listed below, other property owners in the Gateway Area, the Town of Gilbert Staff and the design team who contributed their efforts for these design standards and guidelines to become a reality.

Chuck Backus - ASU East
Cooley Family
McDavid Dobson
Joe Johnston

Barney Family
DiBella Family
Gieszl Family

The Town of Gilbert would also like to thank the City of Mesa for their assistance with the Power Road corridor, making it a unique entry into this important portion of both communities.

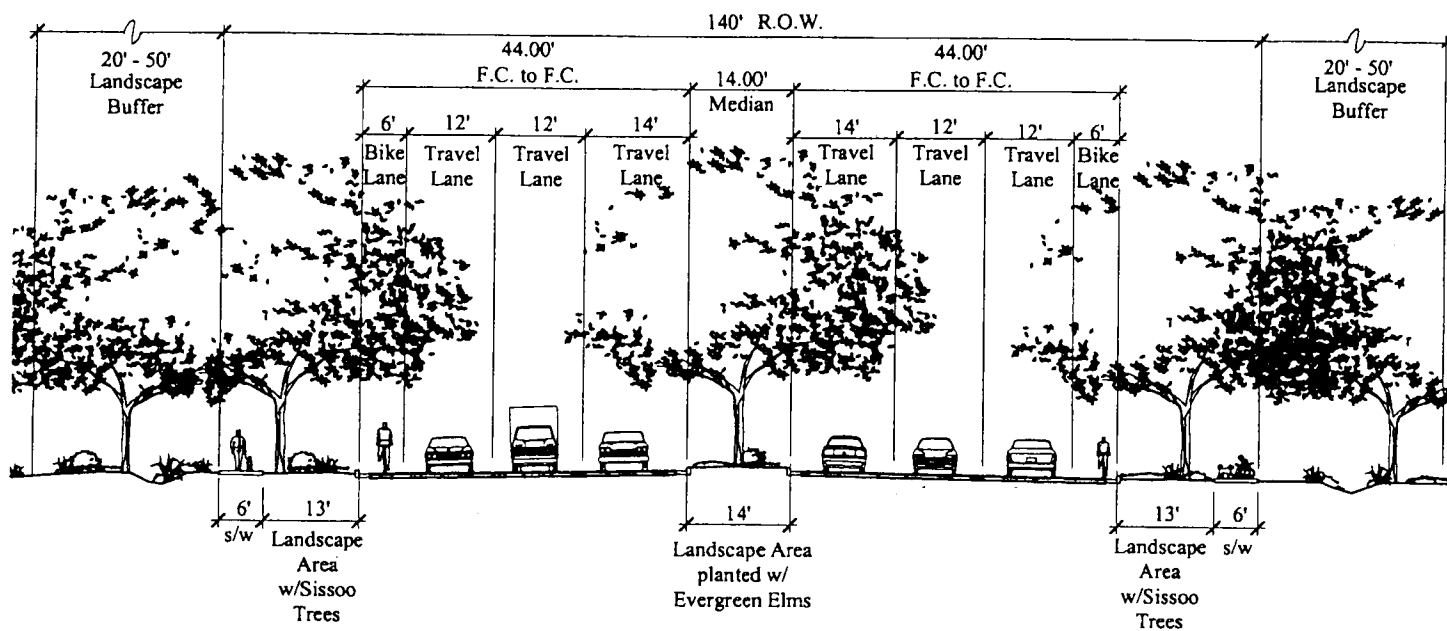
2.0 Street Rights-of-Way

2.1 Power Road

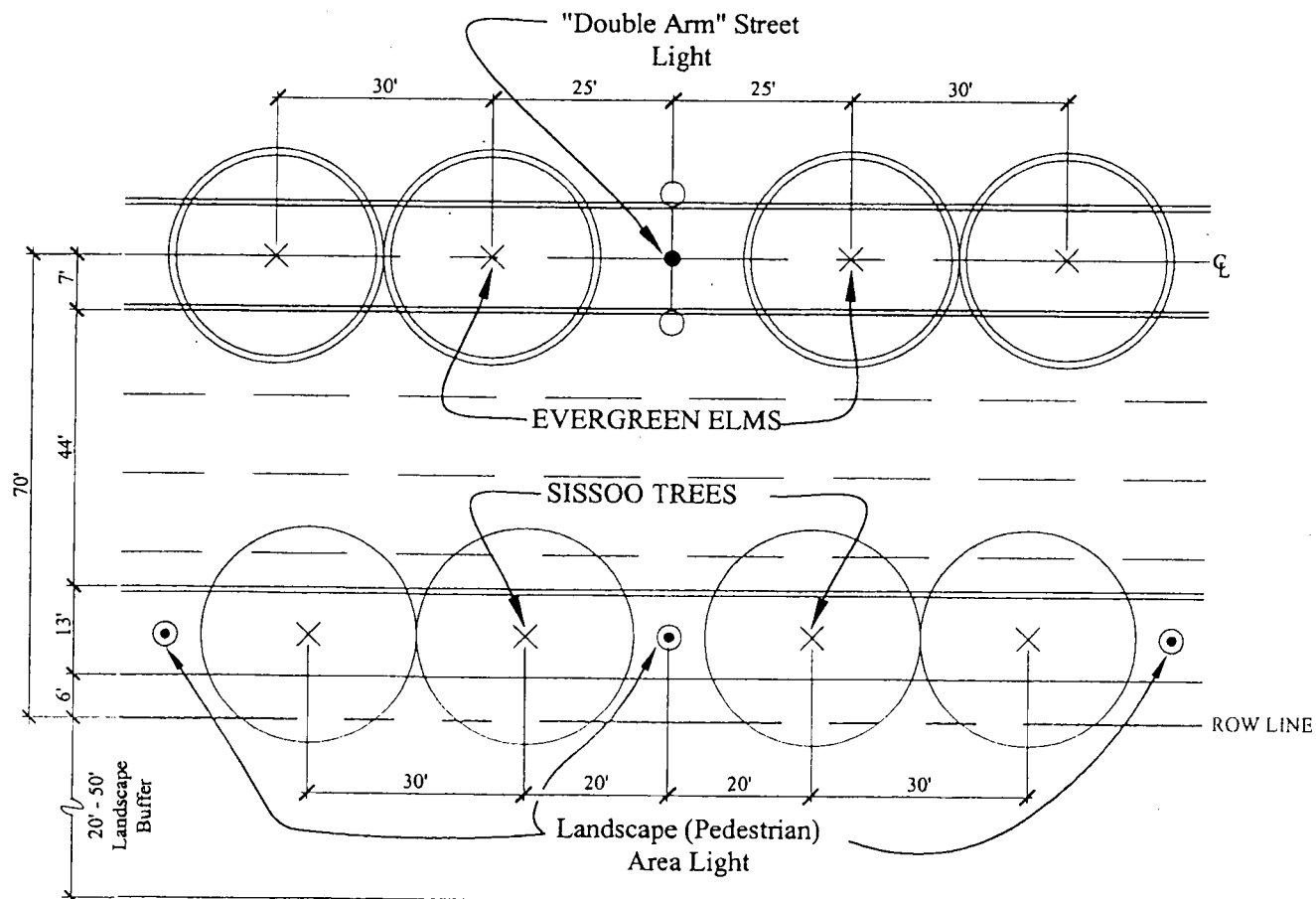
Power Road is designated as a Road of Regional Significance and falls under the Town of Gilbert and City of Mesa within the Gateway Area. The City of Mesa will have jurisdiction for the entire right-of-way of Power Road from Warner Road to the south side of the Williams Field Road intersection. At this point, the entire right-of-way for Power Road falls within the Town of Gilbert's jurisdiction. The following design standards, cross section and layout plan apply to Power Road within Gilbert's jurisdiction. For a description and exhibits relating to intersections within the Gateway Area, refer to Section 3.0. The cross section information is based on Maricopa County standards, which are the standards Gilbert understands are being used for roadway construction. Typically work adjacent to Power Road will be approved by Maricopa County, the City of Mesa and the Town of Gilbert. The landscape buffer easement described below will be required adjacent to the west right-of-way of Power Road when the road is outside the Town of Gilbert's jurisdiction. Right-of-way width shall be 140 feet. Posted speed limit for Power Road within the Gateway Area shall be 45 miles per hour. See Figure 1.

1. The roadway shall contain a fourteen-foot (14) wide raised median.
2. One fourteen-foot (14) wide and two twelve-foot (12) wide travel lanes shall be on each side of the median. One six-foot (6) wide bike lane shall be adjacent to each outside travel lane.
3. A six-foot (6) wide walkway, separated from the curb by a thirteen-foot (13) wide landscape area, shall be on each side of the street. Refer to Section 5.0 for streetscape elements.
4. Median landscaping shall consist of 24" box Evergreen Elm trees, 30 feet on center (50 feet when adjacent to lights), set in a straight line and centered within the median. Low groundcover, shrubs and mulch shall comply with Section 5.0. Median bullnoses shall comply with Section 5.3.
5. Median street lights shall be set in a straight line with the median trees and shall comply with Section 5.4.
6. The landscape area behind the curb shall be planted with 24" box Sissoo trees, 30 feet on center (40 feet when adjacent to lights), set in a straight line eight (8) feet from the face of curb. Low groundcover, shrubs and mulch shall comply with Section 5.0.
7. Landscape (pedestrian) area lights shall be set in a straight line with the trees and shall comply with Section 5.4.
8. A landscape buffer easement, varying from twenty (20) feet to fifty (50) feet in width, shall be provided along each side of the right-of-way. This easement area may contain drainage retention basins and shall contain agricultural vernacular landscaping. See Section 4.0.
9. Traffic signals and traffic signs are detailed in Section 5.5. Crosswalks shall comply with Section 5.3.
10. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
11. Utility and driveway locations shall comply with Section 5.9.

Figure 1
Power Road
Cross Section



Plan View



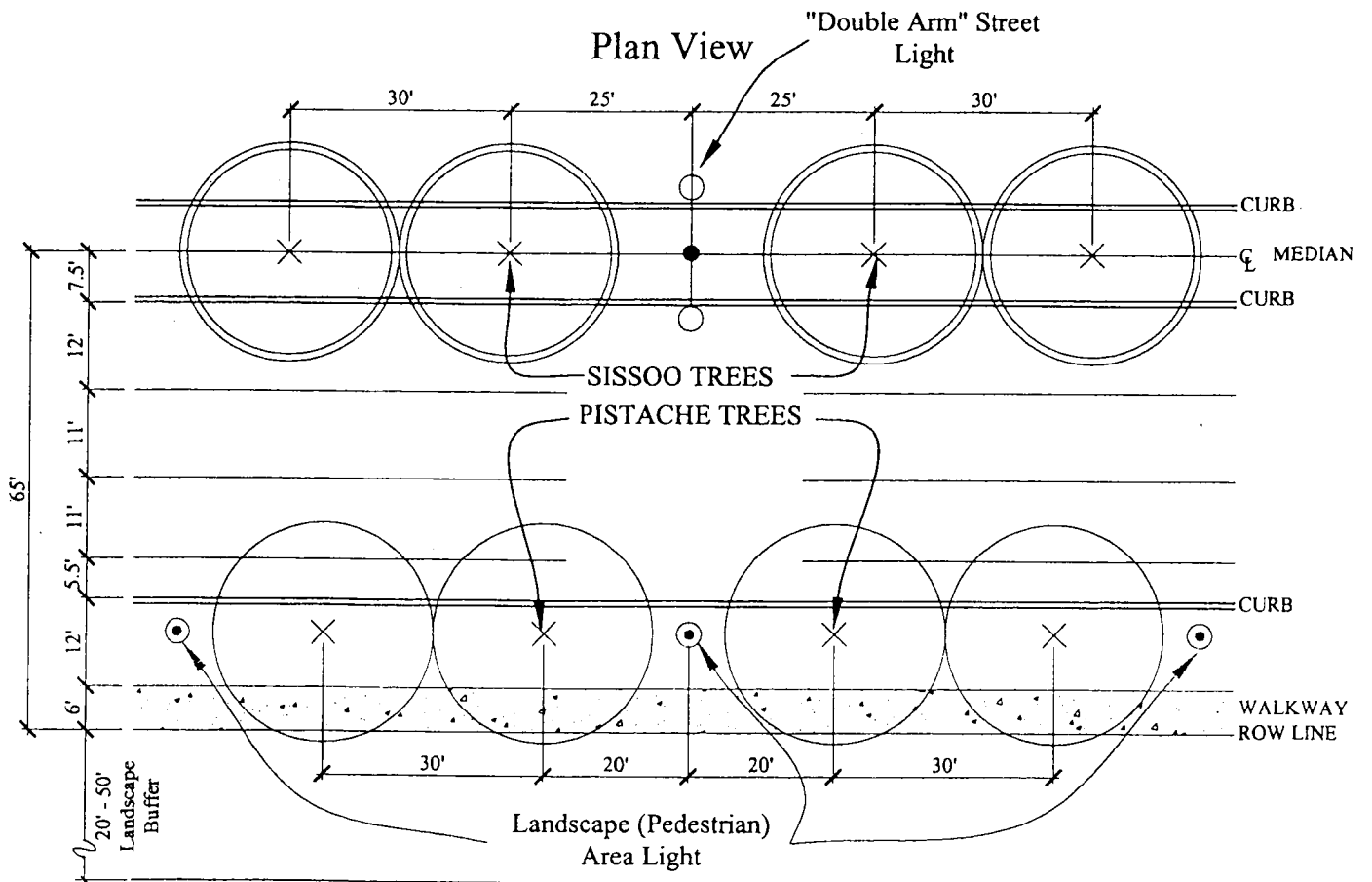
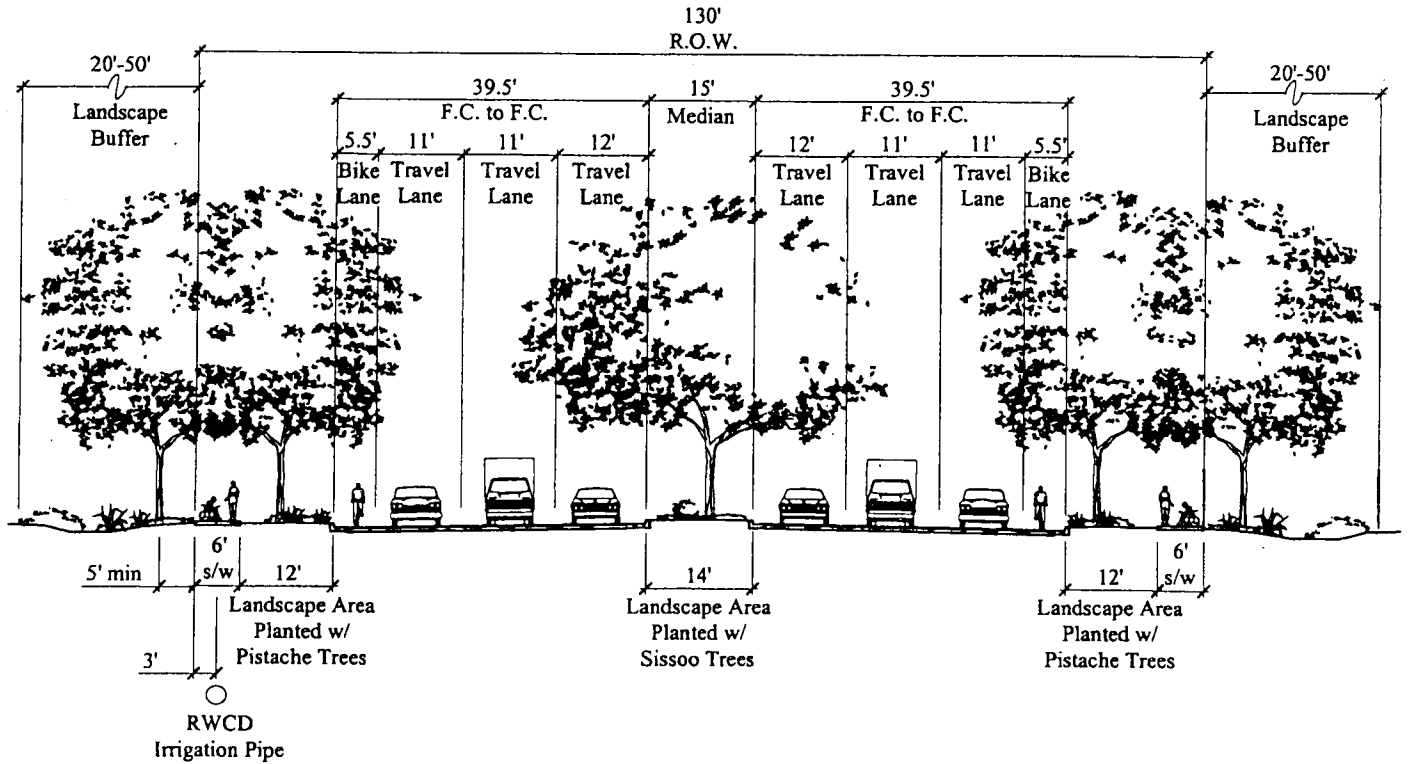
2.2 Higley Road, Warner Road, Ray Road, Pecos Road

Higley Road, Warner Road, Ray Road and Pecos Road are designated as major arterials within the Gateway Area. The following design standards, cross sections and layout plans apply to these roads except at intersections. For descriptions and exhibits relating to intersections, refer to Section 3.0. Right-of-way width shall be 130 feet, or as required by the Town of Gilbert. The posted speed limit on these roads within the Gateway Area shall be 45 miles per hour. See Figure 2.

1. The roadway shall contain a fifteen-foot (15) foot wide raised median.
2. One twelve-foot (12) wide and two eleven-foot (11) wide travel lanes shall be on each side of the median. One five and one-half-foot (5.5) wide bike lane shall be adjacent to each outside travel lane.
3. A six-foot (6) wide walkway, separated from the curb by a twelve-foot (12) wide landscape area, shall be on each side of the street. Refer to Section 5.0 for streetscape elements.
4. Median landscaping shall consist of 24" box Sissoo trees, 30 feet on center (50 feet when adjacent to lights), set in a straight line centered in the median. Low groundcover, shrubs and mulch shall comply with Section 5.0. Median bullnoses shall comply with Section 5.3.
5. Median street lights shall be set in a straight line with the median trees and shall comply with Section 5.4.
6. The landscape area behind the curb shall be planted with 24" box Pistache trees, 30 feet on center (40 feet when adjacent to lights), set in a straight line seven feet (7) from the face of curb. Low groundcover, shrubs and mulch shall comply with Section 5.0.
7. Landscape (pedestrian) area lights shall be set in a straight line with the trees and shall comply with Section 5.4.
8. A landscape buffer easement, varying from twenty (20) feet to fifty (50) feet in width, shall be provided along each side of the right-of-way. This easement area may contain drainage retention basins and shall contain agricultural vernacular landscaping. See Section 4.0.
9. Traffic signals and traffic signs are detailed in Section 5.5. Crosswalks shall comply with Section 5.3.
10. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
11. Utility and driveway locations shall comply with Section 5.9.

Figure 2
Higley Road, Warner Road, Ray Road, Pecos Road

Cross Section

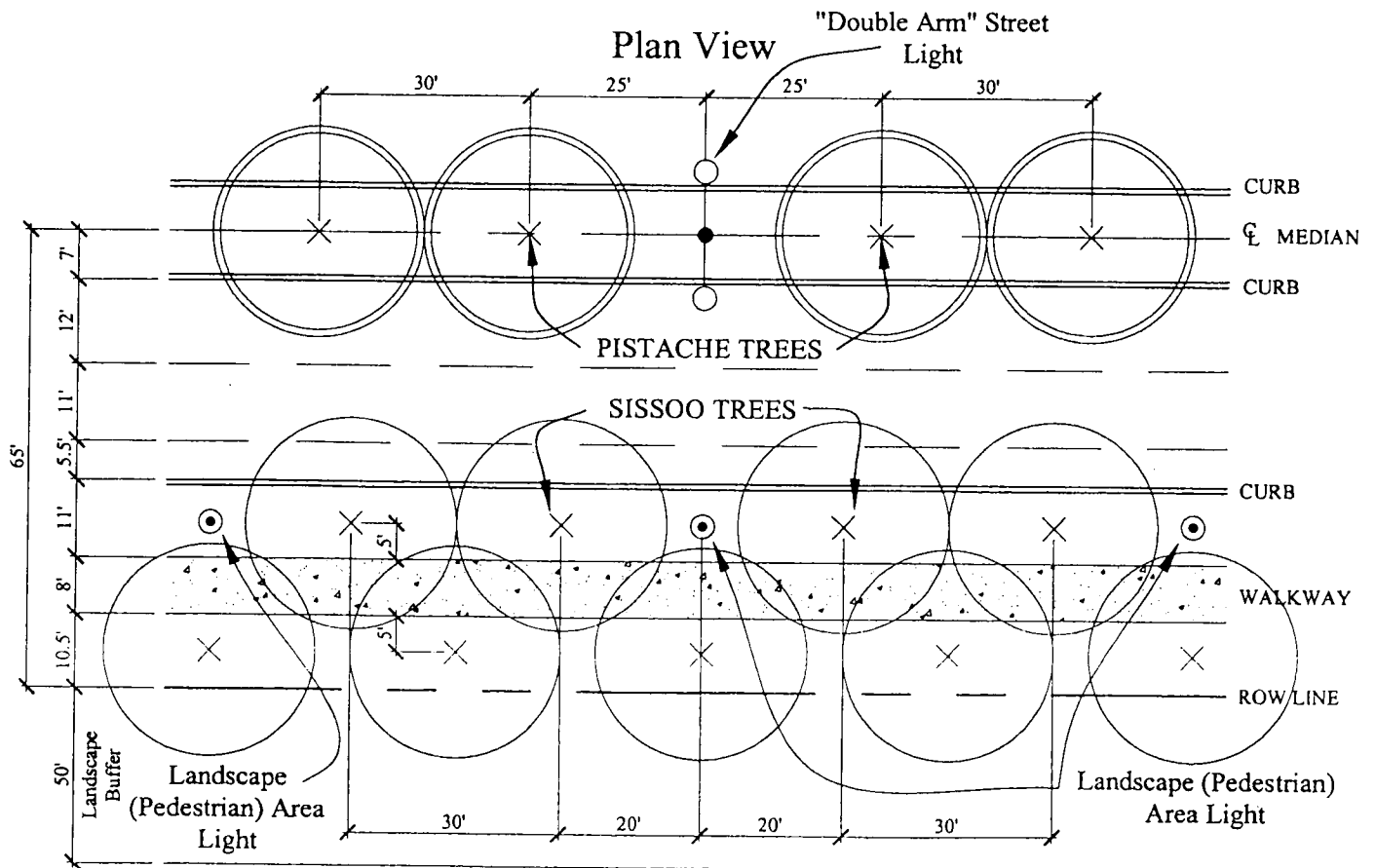
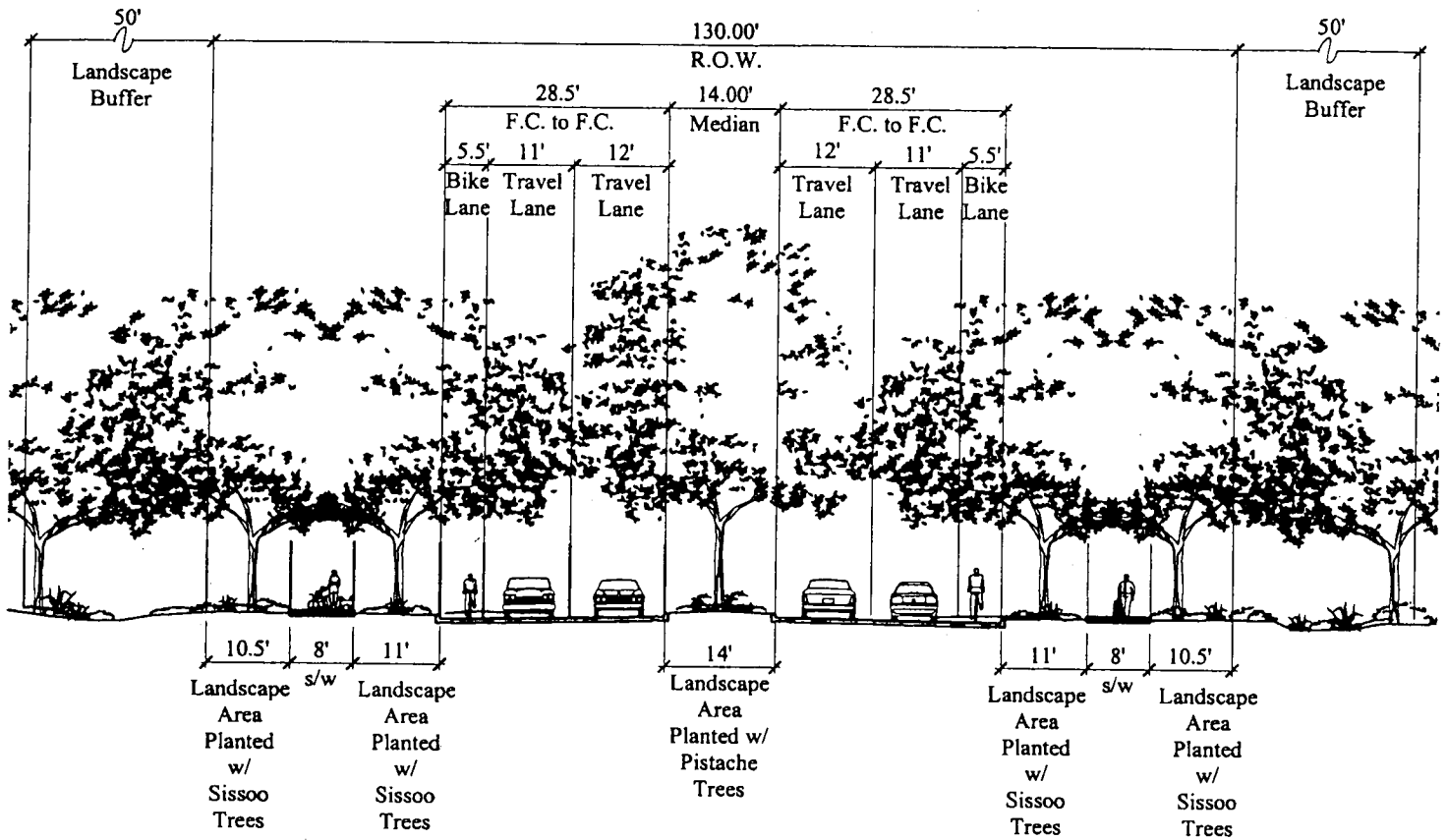


2.3 Williams Field Road

Williams Field Road is designated as a minor arterial from one-fourth (1/4) mile west of Higley Road to Power Road and as a major arterial in the remaining part of the Town. The following design standards, cross section and layout plan apply to this road except at intersections. For descriptions and exhibits relating to intersections, refer to Section 3.0. The General Plan designates approximately 110 acres at the intersection of Williams Field Road and Recker Road as the Village Center (see Section 3.3). Right-of-way width shall be 130 feet. The posted speed limit for Williams Field Road, outside the Village Center, shall be 45 miles per hour. See Figure 3.

1. The roadway shall contain a fourteen-foot (14) wide raised median.
2. One twelve-foot (12) wide and one eleven-foot (11) wide travel lane shall be on each side of the median. One five and one half-foot (5.5) wide bike lane shall be adjacent to each outside travel lane.
3. An eight-foot (8) wide walkway, separated from the curb by an eleven-foot (11) landscape area, shall be on each side of the street. The remaining ten and one-half (10.5) feet shall also be a landscape area. Refer to Section 5.0 for streetscape elements.
4. Median landscaping shall consist of 24" box Pistache trees, 30 feet on center (50 feet when adjacent to lights), set in a straight line centered in the median. Low groundcover, shrubs and mulch shall comply with Section 5.0. Median bullnoses shall comply with Section 5.3.
5. Median street lights shall be set in a straight line with the median trees and shall comply with Section 5.4.
6. The landscape area between the curb and walkway shall be planted with 24" box Sissoo trees, 30 feet on center (40 feet when adjacent to lights), set in a straight line six feet (6) from the face of curb. The landscape area between the walkway and the right-of-way shall be planted with 24" box Sissoo trees, 30 feet on center, in a straight line five (5) feet from the outside edge of the walkway. Low groundcover, shrubs and mulch shall comply with Section 5.0.
7. Landscape (pedestrian) area lights shall be set in a straight line with the trees and shall comply with Section 5.4.
8. A fifty-foot (50) wide landscape buffer easement shall be provided along each side of the right-of-way outside the Village Center. This easement area may contain drainage retention basins and shall contain agricultural vernacular landscaping. Refer to Section 4.0.
9. Traffic signals and traffic signs are detailed in Section 5.5. Crosswalks shall comply with Section 5.3.
10. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
11. Utility and driveway locations and shall comply with Section 5.9.

Figure 3
Williams Field Road
Cross Section



2.4 Recker Road

Recker Road is designated as a minor arterial in the Gateway Area. The following design standards, cross section and layout plan apply to this road except at intersections. For a description of exhibits relating to intersections, refer to Section 3.0. The General Plan designates approximately 110 acres at the intersection of Williams Field Road and Recker Road as the Village Center (see Section 3.3). Right-of-way width shall be 130 feet. The posted speed limit for Recker Road, outside the Village Center, shall be 45 miles per hour. See Figure 4.

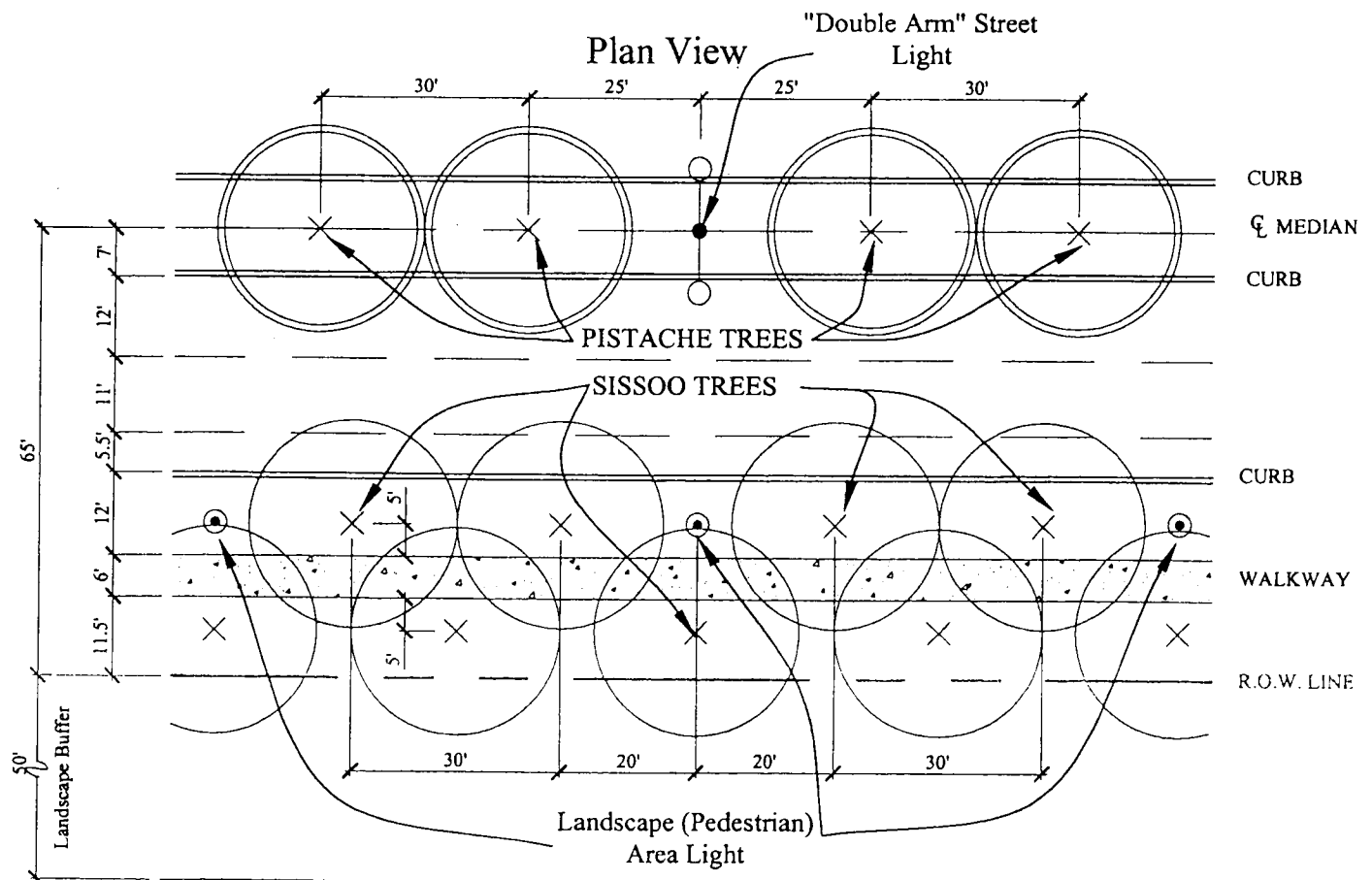
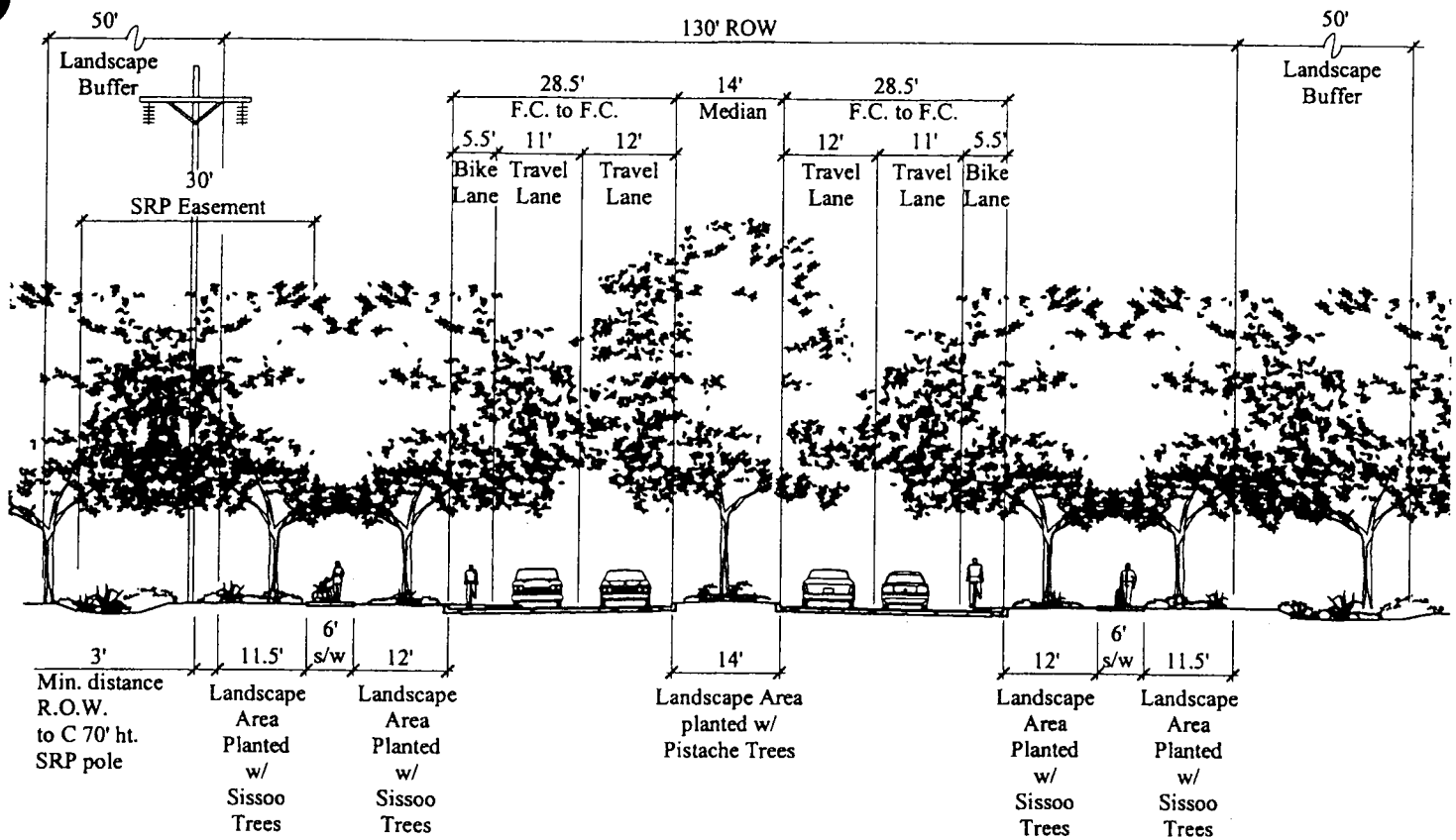
1. The roadway shall contain a fourteen-foot (14) wide raised median.
2. One twelve-foot (12) wide and one eleven-foot (11) wide travel lane shall be on each side of the median. One five and one half-foot (5.5) wide bike lane shall be adjacent to each outside travel lane.
3. A six-foot (6) wide walkway, separated from the curb by a twelve-foot (12) wide landscape area, shall be on each side of the street. The remaining eleven and one-half (11.5) feet shall also be a landscape area. Refer to Section 5.0 for streetscape elements.
4. Median landscaping shall consist of 24" box Pistache trees, 30 feet on center (50 feet when adjacent to lights), set in a straight line centered in the median. Low groundcover, shrubs and mulch shall comply with Section 5.0. Median bullnoses shall comply with Section 5.3.
5. Median street lights shall be set in a straight line with the median trees and shall comply with Section 5.4.
6. The landscape area between the curb and walkway shall be planted with 24" box Sissoo trees, 30 feet on center (40 feet when adjacent to lights), set in a straight line seven (7) feet from the face of curb. The landscape area between the walkway and the right-of-way shall be planted with 24" box Sissoo trees, 30 feet on center, in a straight line five (5) feet from the outside edge of the walkway. Low groundcover, shrubs and mulch shall comply with Section 5.0.
7. Landscape (pedestrian) area lights shall be set in a straight line with the trees and shall comply with Section 5.4.
8. A fifty-foot (50) wide landscape buffer easement shall be provided along each side of the right-of-way outside the Village Center. This easement area may contain drainage retention basins and shall contain agricultural vernacular landscaping. Refer to Section 4.0.
9. Traffic signals and traffic signs are detailed in Section 5.5. Crosswalks shall comply with Section 5.3.
10. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
11. Utility and driveway locations shall comply with Section 5.9.

2.5 Collector and Local Streets

Rights-of-way for collector streets and public local streets shall conform to Town of Gilbert standard details. Landscape materials shall continue the agrarian theme of the Gateway Area, although specific plant materials may vary. Required crosswalks within the Gateway Area shall comply with the design details in Section 5.3. Median bullnoses shall comply with Section

5.3, where medians are used. Collector streets shall continue the lighting theme by using the single-arm street light. Landscape (pedestrian) lights may be included along Collector Roads as approved by the Town of Gilbert. All lights shall comply with Section 5.4. Street and traffic signs shall be consistent throughout the Gateway Area, and shall conform to the applicable standards in Section 5.0.

The General Plan states that local streets shall provide large, agricultural vernacular, canopy trees along both sides of each street, planted between curbs and walkways within landscape strips. Low scale street lighting may be provided in this landscape strip, and where provided, shall conform to lights in Section 5.4.



3.0 Intersections

Major intersections in the Gateway Area consist of Town Entry Intersections (Section 3.1), Arterial Intersections (Section 3.2), the Village Center (Section 3.3), Collector and Local Street Intersections (Section 3.4). All intersections shall comply with Town of Gilbert Standard Details 92 and 93.

3.1 Town Entry Intersections

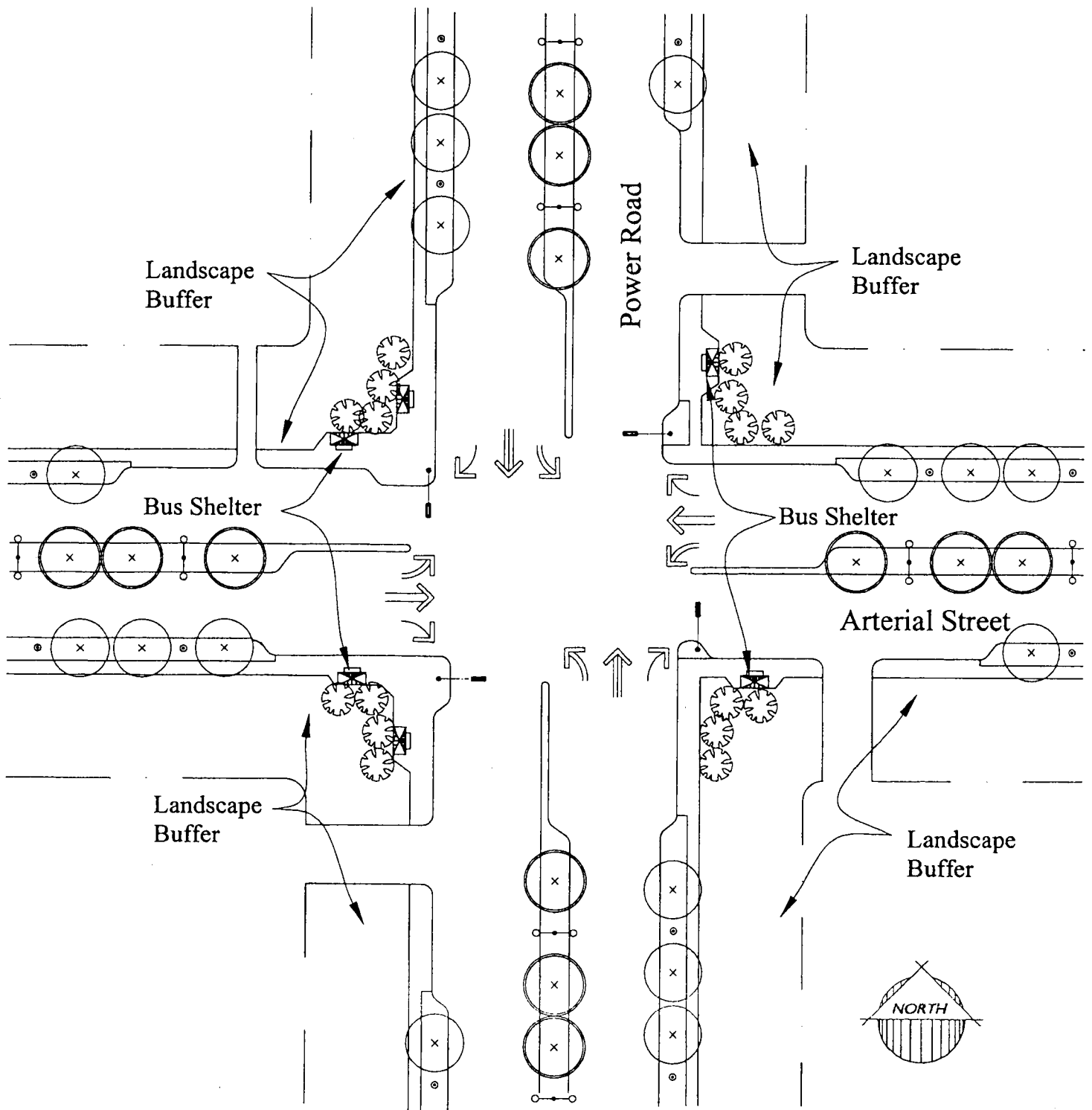
Town Entry intersections occur along Power Road at intersections with Warner Road, Ray Road and Williams Field Road. The following design standards and layout plan are applicable to these intersections. See Figure 5.

1. Warner Road and Ray Road shall have two twelve-foot (12) wide left-turn lanes, three through travel lanes, one bike lane, one twelve-foot (12) wide right-turn lane and walkway. Williams Field Road shall have a twelve-foot (12) wide left-turn lane except at Power Road where there shall be two twelve-foot (12) wide left turn lanes, two through travel lanes, one bike lane, one twelve-foot (12) wide right-turn lane and walkway. The travel lanes, bike lane and walkway shall be consistent with the applicable roadway cross sections in Section 2.0. The walkway shall be expanded to back of curb at the end of the right-turn lane transition.
2. Within the Town of Gilbert's jurisdiction, Power Road shall have two left-turn lanes (a twelve-foot (12) wide lane adjacent to the curb and an eleven-foot (11) wide interior lane), three through travel lanes, one bike lane, one twelve-foot (12) wide right-turn lane and walkway. The travel lanes, bike lane and walkway shall be consistent with the Power Road cross section in Section 2.1. The walkway shall be expanded to back of curb at the end of the right-turn lane transition.
3. Median tree plantings and lights for Warner Road, Ray Road, Williams Field Road and Pecos Road shall be located to allow proper visibility of all traffic signal indications at the intersection. Trees and lights shall conform to the design standards for each road in Section 2.0. Median bullnoses shall comply with Section 5.3.
4. Within the Town of Gilbert's jurisdiction, median tree plantings and lights for Power Road shall be located to allow proper visibility of all traffic signal indications at the intersection. Trees and lights shall conform to the design standards for Power Road in Section 2.1. Median bullnoses shall comply with Section 5.3.
5. Landscape areas between the curb and walkway for Warner Road, Ray Road, Williams Field Road and Pecos Road shall conform to the applicable design standards for each road. Trees shall be located to allow proper visibility of all traffic signal indications at the intersection.
6. Within the Town of Gilbert's jurisdiction, the landscape area between the curb and walkway for Power Road shall conform to the design standards for Power Road in Section 2.1. Trees shall be located to allow proper visibility of all traffic signal indications at the intersection.
7. Where not already provided, a fifty-foot (50) wide landscape buffer easement shall be required adjacent to the right-of-way at each intersection. This easement shall

- extend 250 feet from the intersection as measured from the centerline of the intersection. Design standards are contained in Section 4.0.
8. Street light locations shall be designed by a registered civil engineer and submitted for review with the construction documents, including photometrics.
 9. Landscape (pedestrian) area lights shall comply with applicable design standards for the specific roadway and with Section 5.4.
 10. Traffic signals and traffic signs shall comply with Section 5.5.
 11. Crosswalks shall be provided as required by the Town and shall comply with design standards contained in Section 5.3.
 12. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
 13. A Town Entry Monument shall be provided within the landscape buffer easement on the north side of Warner Road, Ray Road, Williams Field Road and Pecos Road. The monument shall contain the Town of Gilbert logo and incorporate the word "Welcome" into the design. Accent lighting shall be provided to illuminate the entry monument at night. Entry monuments shall be integrated into the overall design of the project. Designs shall be submitted and reviewed through the Town of Gilbert Design Review Board. Entry monuments shall be installed outside sight lines as outlined in Town of Gilbert Standard Details 92 and 93.
 14. Agrarian heritage elements may be placed within the landscape buffer easement at Arterial Intersections. The feature shall be fixed in place on a paved plaza area that is connected to the arterial street walkways and pedestrian plazas. Accent lighting shall be provided for night illumination. An interpretive sign may be provided which describes the history of the particular heritage item. Designs for this feature shall be submitted and reviewed through the Town of Gilbert Design Review Board.
 15. Utility and driveway locations shall comply with Section 5.9.

Figure 5
Town Entry Intersections

Plan View



3.2 Arterial Intersections

Arterial intersections occur at one-mile intervals within the Gateway Area and include: Warner Road/Recker Road; Warner Road/Higley Road; Ray Road/Recker Road; Ray Road/Higley Road; Williams Field Road/Higley Road; Pecos Road/Recker Road; and Pecos Road/Higley Road. Due to existing businesses, the intersection of Williams Field Road and Higley Road is addressed separately. The following design standards and layout plans are applicable to these intersections. See Figure 6.

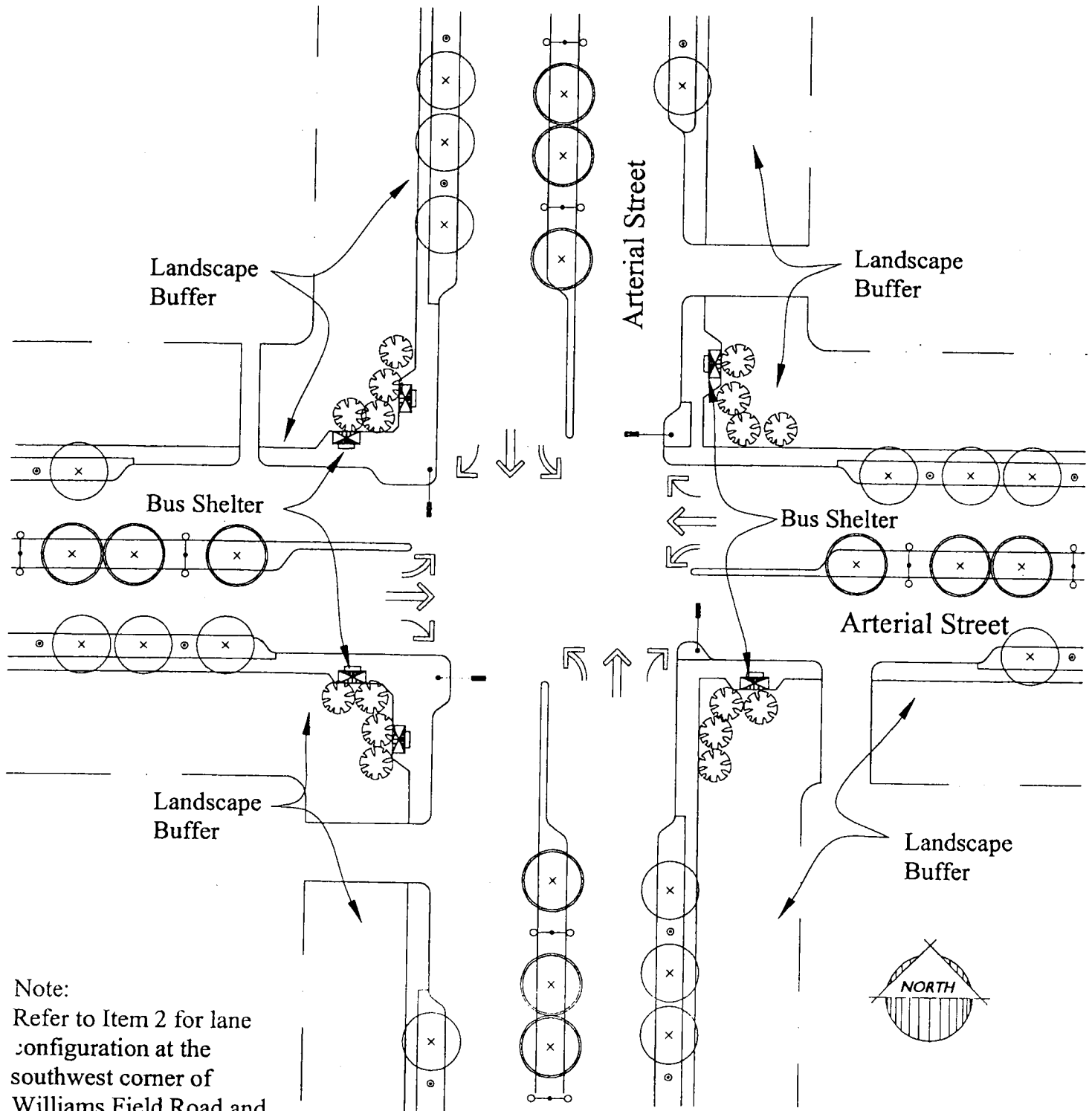
1. Warner Road, Ray Road, Pecos Road and Higley Road shall have two twelve-foot (12) wide left-turn lanes, three through travel lanes, one bike lane, one twelve-foot (12) wide right-turn lane and walkway. Recker Road shall have a twelve-foot (12) wide left-turn lane, two through travel lanes, one bike lane, one twelve-foot (12) wide right-turn lane and walkway. The travel lanes, bike lane and walkway shall be consistent with the applicable roadway cross section in Section 2.0. The walkway shall be expanded to back of curb at the end of the right-turn lane transition.
2. Protection of the existing small businesses at the southeast corner of the intersection of Williams Field Road and Higley Road requires two modifications from the other arterial intersections. No right-turn lane shall be provided on Williams Field Road for southbound traffic on Higley Road. Similarly, Higley Road shall have only two through travel lanes south of the intersection until there is adequate room to transition to the third travel lane. Bike lanes, walkways and landscape areas shall also accommodate the existing businesses. If these businesses cease to exist in the future, then the intersection shall comply with the design standards and layout plans for the roadways and arterial intersections.
3. Median tree plantings and lights shall be located to allow proper visibility of the intersection. Trees and lights shall conform to the design standards for each road in Section 2.0.
4. Landscape areas between the curb and walkway shall conform to the applicable design standards for each road. Trees shall be located to allow proper visibility of all traffic signal indications at the intersection.
5. Where not already provided, a fifty-foot (50) wide landscape buffer easement shall be required adjacent to the right-of-way at each intersection. This easement shall extend 250 feet from the intersection as measured from the centerline of the intersection. Design standards are contained in Section 4.0.
6. Street light locations shall be designed by a registered civil engineer and submitted for review with the construction documents, including photometrics.
7. Landscape (pedestrian) area lights shall comply with applicable design standards for the specific roadway and with Section 5.4.
8. Traffic signals and traffic signs shall comply with Section 5.5.
9. Crosswalks shall be provided as required by the Town and shall comply with design standards contained in Section 5.3.
10. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
11. Agrarian heritage elements may be placed within the landscape buffer easement at Arterial Intersections. The feature shall be fixed in place on a paved plaza area that is connected to the arterial street walkways and pedestrian plazas. Accent lighting

shall be provided for night illumination. An interpretive sign may be provided which describes the history of the particular heritage item. Designs for this feature shall be submitted and reviewed through the Town of Gilbert Design Review Board.

12. Utility and driveway locations shall comply with Section 5.9.

Figure 6
Arterial Intersections

Plan View



Note:
Refer to Item 2 for lane
configuration at the
southwest corner of
Williams Field Road and
Higley Road.

3.3 Village Center

The General Plan designates approximately 110 acres at the intersection of Williams Field Road and Recker Road as the Village Center. The following design standards and layout plan apply to this intersection. The right-of-way width for these portions of Williams Field Road and Recker Road shall be variable and subject to approval by the Town. The posted speed limit within the Village Center shall be as determined by the Traffic Engineer. See Figures 7 and 8.

1. The roadways shall contain a raised median, with minimum width of fourteen (14) feet. One twelve-foot (12) wide and one eleven-foot (11) wide travel lane shall be on each side of the median. One five and one-half (5.5) foot wide bike lane shall be adjacent to each outside travel lane.
2. Left- and right-turn lanes shall be provided for each direction of travel. The turn lanes shall be twelve (12) feet wide.
3. The area behind the curb line shall contain a twelve-foot (12) wide on-street parking lane, landscape areas and pedestrian plazas. On-street parking lanes shall be a maximum of seventy-five (75) feet in length and shall be separated by landscape areas/pedestrian plazas that are a minimum of twenty-five (25) feet in length. Pedestrian walkways and plaza areas shall be a minimum fifteen (15) feet in width.
4. Travel lanes may become separated by an expanded median, resulting in one-way traffic within a landscaped, pedestrian boulevard. As streets become one-way, the right-of-way shall correspond to the reduced roadway section. At a minimum, one twelve-foot (12) wide and one eleven-foot (11) wide through travel lane, one five and one-half foot (5.5) wide bike lane and one twelve-foot (12) wide on-street parking lane will be maintained. An additional on-street parking lane may be provided along one-way sections of Higley Road. One-way intersections of Higley Road with Williams Field Road shall be signalized to operate as a single traffic control signal and shall be tied together. Minimum spacing for independently operating traffic signals shall be one-fourth (1/4) mile from the centerline of the intersection of Williams Field Road and Higley Road. Final design shall be approved by the Town of Gilbert Traffic Engineering Department.
5. Median landscaping and street lights shall comply with design standards in Section 2.0 for the applicable roadway. Trees and lights shall be located to allow proper visibility of all traffic signal indications at the intersection.
6. Landscape areas between the curb and the walkway shall conform to design standards for the applicable roadway contained in Section 2.0. Walkways shall be a minimum of fifteen (15) feet in width and may be expanded to create pedestrian plazas. Additional street trees may be planted in pedestrian plazas and landscape areas. Trees shall be located to allow proper visibility of the intersection.
7. Landscape (pedestrian) area lights shall comply with applicable design standards for the specific roadway and with Section 5.4.
8. Traffic signals and traffic signs shall comply with Section 5.5.
9. As this intersection is the heart of the Village Center, special treatment of vehicular and pedestrian pavement areas shall be required. This treatment can take the form of colored, textured concrete; interlocking pavers; colored asphalt or other creative forms. Specialty pavement must meet ADA requirements.

10. At a minimum, crosswalks shall be twelve (12) feet in width with an additional ten-inch (10") wide concrete restraint curb before and after the crosswalk. Material and design details are contained in Section 5.3. Crosswalks shall be provided as required by the Town.
11. A standard, RPTA mid-block bus bay shall be provided per Section 5.6.
12. Utility and driveway locations shall comply with Section 5.9.

RECKE ROAD

WILLIAMS FIELD ROAD

ELAND STREET

TRAIL

CIVIC GREEN

TRANSIT STATION

RES. OVER RETAIL

PARK

RETAIL OR LIVE/WORK

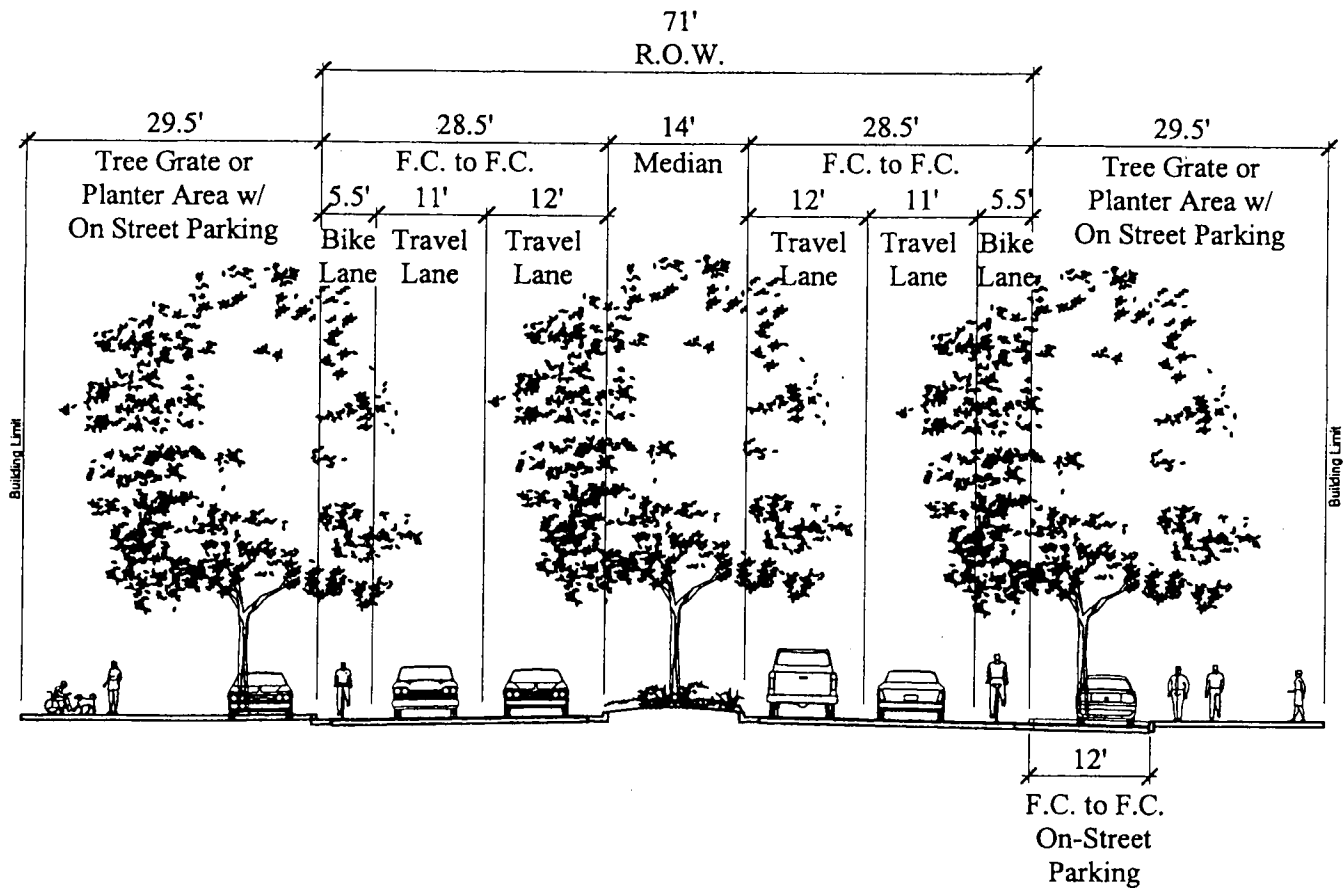
EMPLOYMENT

RESIDENTIAL

25

Figure 8
 Village Center Arterial Streets

Cross Section



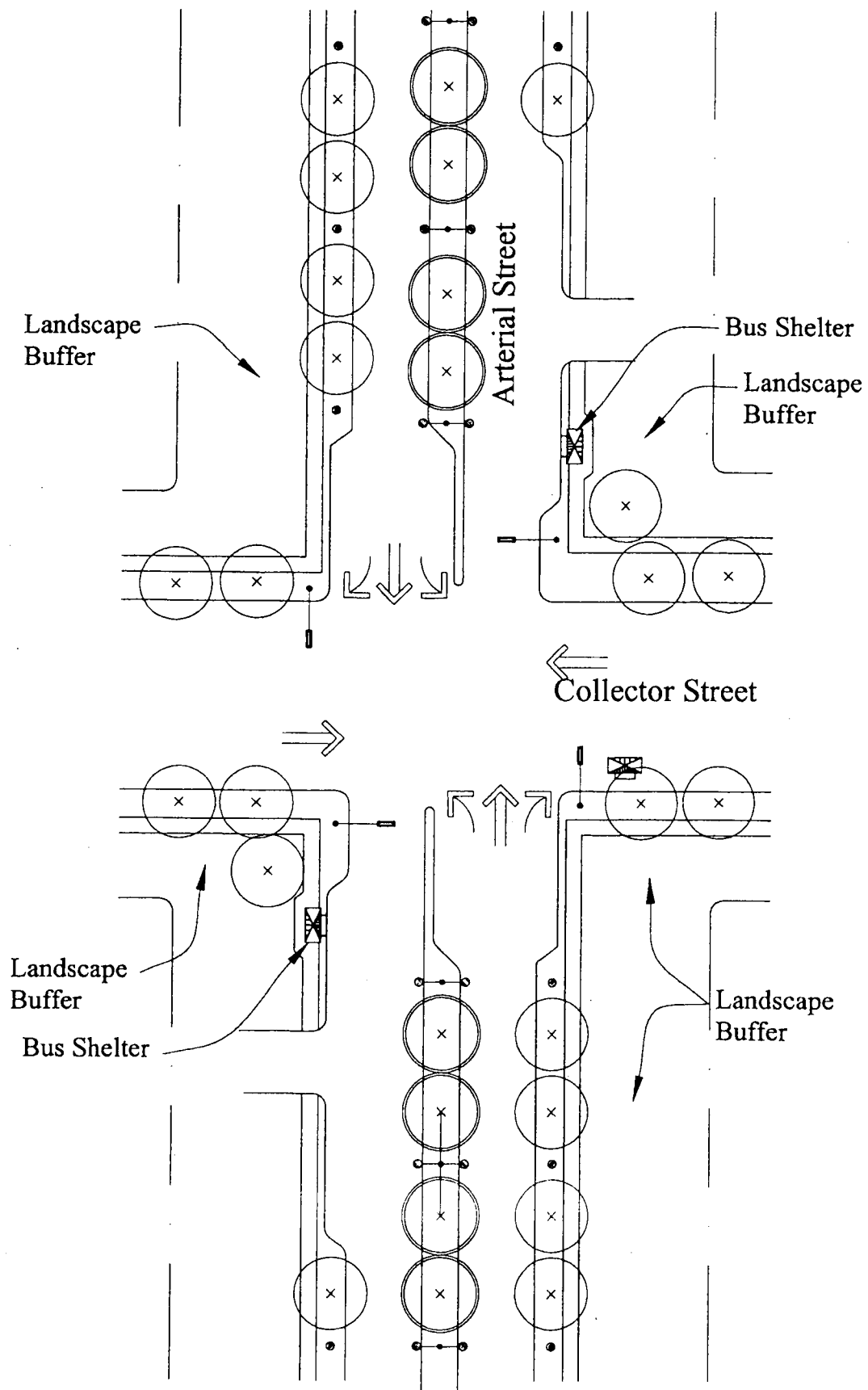
3.4 Collector and Local Street Intersections

Collector and local street intersections will occur throughout the Gateway Area. Continuation of the Gateway Area streetscape, including street and pedestrian-scale lights is required along collector streets. Typically collector street intersections will be signalized, and local street intersections will be controlled with a stop sign on the local street. Posted speed limits for local and collector streets will be per the Town of Gilbert Engineering Department. See Figure 9.

1. For collector street/arterial street intersections, twelve-foot (12) wide left- and right-turn lanes will be provided in the arterial roadway. Through travel lanes, bike lanes and walkways shall be consistent with those in the applicable arterial cross section. The walkway along the arterial shall be expanded to the back of curb at the end of the right-turn lane transition. Collector streets shall conform to Town of Gilbert standard details, or may be enhanced as approved by the Town.
2. For local street/arterial street intersections and subdivision entryway/arterial street intersections, a twelve-foot (12) wide deceleration lane shall be provided along the arterial street. Through travel lanes, bike lanes and walkways shall be consistent with those in the applicable arterial cross section. A minimum four-foot (4) wide walkway shall be provided along both sides of the local street or subdivision entryway, separated from the curb by a landscape area that shall be a minimum of four (4) feet wide. A raised landscape median may be provided in the local street or subdivision entryway. If a landscape median is provided, it shall be a minimum of ten (10) feet wide and the minimum roadway width (to face of curb) on each side of the median shall be twenty (20) feet.
3. Landscape materials for collector and local streets shall continue an agrarian theme. As possible, street trees along collector and local streets shall reflect those on adjacent arterials.
4. Collector street lights and landscape (pedestrian) area lights shall conform to applicable standards in Section 5.4. Street lights shall be sited per the Town of Gilbert Traffic Engineer. Landscape (pedestrian) area lights shall be located within landscape areas as necessary. Local street lights may continue the Gateway theme, but are not required.
5. Traffic signals and traffic signs shall comply with Section 5.5.
6. Crosswalks shall be provided as required by the Town and shall comply with design standards contained in Section 5.3.
7. A standard, RPTA mid-block bus bay may be provided at the far side of a signalized, one-half mile intersection, as approved by the RPTA. If provided, the bus bay shall conform to design standards in Section 5.3.
8. Utility and driveway locations shall comply with Section 5.9.

Figure 9
Collector Street Intersection

Plan View



4.0 Landscape Buffer Easements

Landscape buffer easements are required throughout the Gateway Area with the purpose of emphasizing Gilbert's agrarian heritage. The two types of landscape buffer easements, as well as the locations of each type, are described in this section. All elements within these easements shall comply with sight distance requirements contained in Town of Gilbert Standard Details 92 and 93.

A variable-width landscape buffer easement is required adjacent to the rights-of-way of Power Road, Higley Road, Warner Road, Ray Road and Pecos Road. This easement shall vary in width between twenty (20) and fifty (50) feet, and shall be provided adjacent to the arterial street right-of-way.

A fifty-foot (50') wide landscape buffer easement is required adjacent to the rights-of-way of Williams Field Road and Recker Road (outside the Village Center), at Town Entry Intersections and at Arterial Street Intersections.

Both types of landscape buffer easements shall conform to the following requirements.

1. The buffer area shall be graded to serve as a storm drainage retention basin at its widest points.
2. Agrarian vernacular trees, shrubs and groundcover contained in Section 5.1 shall be planted within the easement. Minimum plant sizes are to be 24-inch box for trees, 5-gallon for shrubs and one-gallon for groundcover. When mature, shrubs and groundcovers shall provide seventy-five percent (75%) ground coverage of the landscape area.
3. Pedestrian and bicycle access ways shall be provided through the buffer areas at appropriate locations to access arterial walkways and transit stops.
4. Utility locations shall comply with Section 5.9.

5.0 Streetscape Elements

5.1 Landscape Materials

The plant materials identified in this section are compatible with the agrarian-oriented theme of the Gateway Area and shall be used on projects in this part of the Town. Other plant species that are compatible with the design concepts and character of the Gateway Area and are not on this list will be considered. The evaluation of additional plants will be based on plant availability, climatic adaptability, landscape maintenance requirements and suitability for the agricultural vernacular theme. Plant materials used within the rights-of-way must be on the Arizona Department of Water Resources Low Water Use Plant List for the Phoenix Active Management Area.

1. Specific tree planting requirements for street rights-of-way are contained in Section 2.0.
2. Shrubs and low ground cover plants shall be included in all landscape areas. When mature, shrubs and groundcovers shall provide seventy-five percent (75%) ground coverage of the landscape area.
3. Dark brown decomposed granite mulch shall be applied to all landscape areas, including medians and landscape easements. Mulch shall be 1/2" or 5/8" and may be either minus or screened. Mulch shall be a minimum depth of two (2) inches. Samples shall be submitted to the Town for approval.
4. Specific tree planting requirements for landscape buffer easements are contained in Section 4.0.
5. All required arterial right-of-way theme trees are to be matching specimen, single trunk and a minimum 24-inch box size when planted. This is defined as a full-bodied tree with a shape characteristic of the species and with a minimum size of ten (10) to twelve (12) feet in height, six (6) to eight (8) feet in width with a two (2) inch single trunk caliper.
6. Figure 10 contains the list of required and recommended plant materials.

Figure 10
Required and Recommended Plant Material

Required Arterial Right-of-Way Trees

Dalbergia sissoo
Pistacia chinensis
Ulmus parvifolia

Sissoo Tree
Chinese Pistache
Evergreen Elm

Recommended Buffer Area Trees

Chitalpa tashkentensis
Cordia boissieri
Fraxinus greggii
Pistacia lentiscus
Pinus eldarica
Quercus virginiana
Schinus terebinthifolius
Vitex agnus-castus

Chitalpa Tree
Texas Olive
Littleleaf Ash
Mastic Tree
Mondel Pine
Live Oak
Brazilian Pepper Tree
Chaste Tree

Recommended Buffer Area Shrubs

Calliandra 'Maricopa Red'
Cordia parvifolia
Dalea versicolor
Eremophila spp. 'Valentine'
Leucophyllum langmaniae
Myrtus spp.
Nerium oleander 'Petite Pink'
Punica spp.
Pyracantha spp.
Ruellia brittoniana
Xylosma spp.

Maricopa Red Calliandra
Littleleaf Cordia
Versicolor Dalea
Valentine Eremophila
Rio Bravo Sage
Myrtle
Petite Pink Oleander
Pomegranite
Pyracantha
Purple Ruellia
Xylosma

Recommended Accent Plants

Muhlenbergia spp.

Deer Grass

Recommended Groundcover Plants

Baccharis hybrid 'Starn'
Dalea capitata 'Sierra Gold'
Hymenoxys acaulis
Lantana spp.
Myoporum parvifolium 'Dwarf'
Rosmarinus officinalis 'Prostratus'
Ruellia brittoniana 'Katie'
Verbena peruviana
Wedelia trilobata

Thompson Baccharis
Sierra Gold Dalea
Angelita Daisy
Lantana
Dwarf Myoporum
Dwarf Rosemary
Katie Ruellia
Peruvian Verbena
Yellow Dot

5.2 Landscape Maintenance

The landscape plant palette for the Gateway Area was selected based on an agrarian theme. Proper maintenance of these living materials is critical to establishing the desired appearance, texture and seasonal color consistent with this theme. All plant materials shall have a permanent, automatic, underground source of irrigation. To this end, the following specifications for landscape maintenance have been developed.

1. All landscape areas adjacent to street intersections shall conform to Town of Gilbert Standard Detail 93 to maintain proper sight distance.
2. Trimming/pruning of individual plant materials shall occur just once a year and at a specified time of the year. The once a year trimming/pruning will allow each plant to recover while retaining the plant's natural size, shape, leaf texture and seasonal color. If growth of plant material is such that it needs a light trimming, then this can be accomplished for times of adjustment such as after a storm. If growth is fast and rank, then the irrigation controller needs adjustment so as to not allow excessive growth and water waste.
3. Trees shall be pruned to promote the health of the plant, and to reduce hazards to people and vehicular traffic by training the tree branches to raise the crown in time, so that the lower scaffold branches are no lower than 9 feet at the trunk. This is especially important for the branches that will hang into street areas. See Figure 11. One scaffold branch shall be pruned each year over several dormant seasons rather than removing them all at once. The lowest permanent branches should have wide angles and be less than half the width of the trunk at the point of attachment. Clean cuts outside the branch collar will allow more rapid recovery.
4. Pruning of trees shall be set based on the type of tree involved. Deciduous trees shall be pruned during dormancy when all of the foliage has dropped. Evergreen trees shall be pruned in late winter or early spring. Trees shall not be topped, as this is detrimental to all species.
5. Shrubs are to be maintained with a full branching pattern with no allowances for "balling," "cubing," and such practices. Shrubs shall be pruned once a year and only after the seasonal blooming cycle has finished. See Figure 12.
6. Groundcovers are to be grown so that each plant fills in with adjacent plants. Plants are not to be trimmed individually. Where necessary adjacent to streets or sidewalks, tops of groundcovers are to be trimmed at an angle instead of square trimmed. See Figure 12. Thinning of groundcovers shall only occur as necessary and only along the final two (2) feet from the edges of a contiguous group planting.
7. All landscape areas are to be maintained in accordance with the approved landscape plan and the original plant palette. All such landscape areas are to be kept free of trash, debris and weeds. Dead plant material must be removed and replaced with plants of the same variety and the size as those adjacent to it.
8. Following a yearly landscape maintenance plan will help reduce annual landscape maintenance costs by saving water and pruning time. The focus of this regimen is on trash pick up and seasonal irrigation controller adjustments rather than wasteful monthly pruning which destroys the natural shape and flowering

capacity of the plant materials, promotes woody growth and requires higher water utilization.

9. Additional information on proper pruning techniques is available from the University of Arizona Cooperative Extension. For more information visit www.ag.arizona.edu.
10. The Arizona Department of Water Resources (ADWR) only allows the use of plant species listed on its Low Water Use Plant list in rights-of-way, unless the right-of-way is associated with a residential lot. The Town allows high water use plants, including turf grass, to be planted in the right-of-way if associated with a residential lot along a local street (as detailed in the Unified Land Development Code), subject to the following conditions:
 - a. If reclaimed water or other non-potable supply is, or will ultimately be, the source of water for irrigation of the development's common areas, the irrigation system for which will not be used for irrigation of the landscape strip between the back of curb and the separated sidewalk, regardless of what entity maintains the landscaping.
 - b. If the source of water is potable, and it is supplied by any entity other than the residential lot owner, the landscape tract between the back of curb and the sidewalk must be landscaped with plants on the ADWR Low Water Use Plant list, regardless of whether the landscape is maintained by the Town, the HOA or the residential lot owner. If the source of water is potable and is provided by the residential lot owner, then any type of plant material can be used, regardless of what entity maintains the landscaping.
 - c. If the irrigation water is to be supplied by any entity other than the residential lot owner, the area between the back of curb and the separated sidewalk will be included in the calculation of "landscapable area" for the purposes of compliance with the conservation requirements associated with residential subdivisions.

Figure 11
Tree Pruning

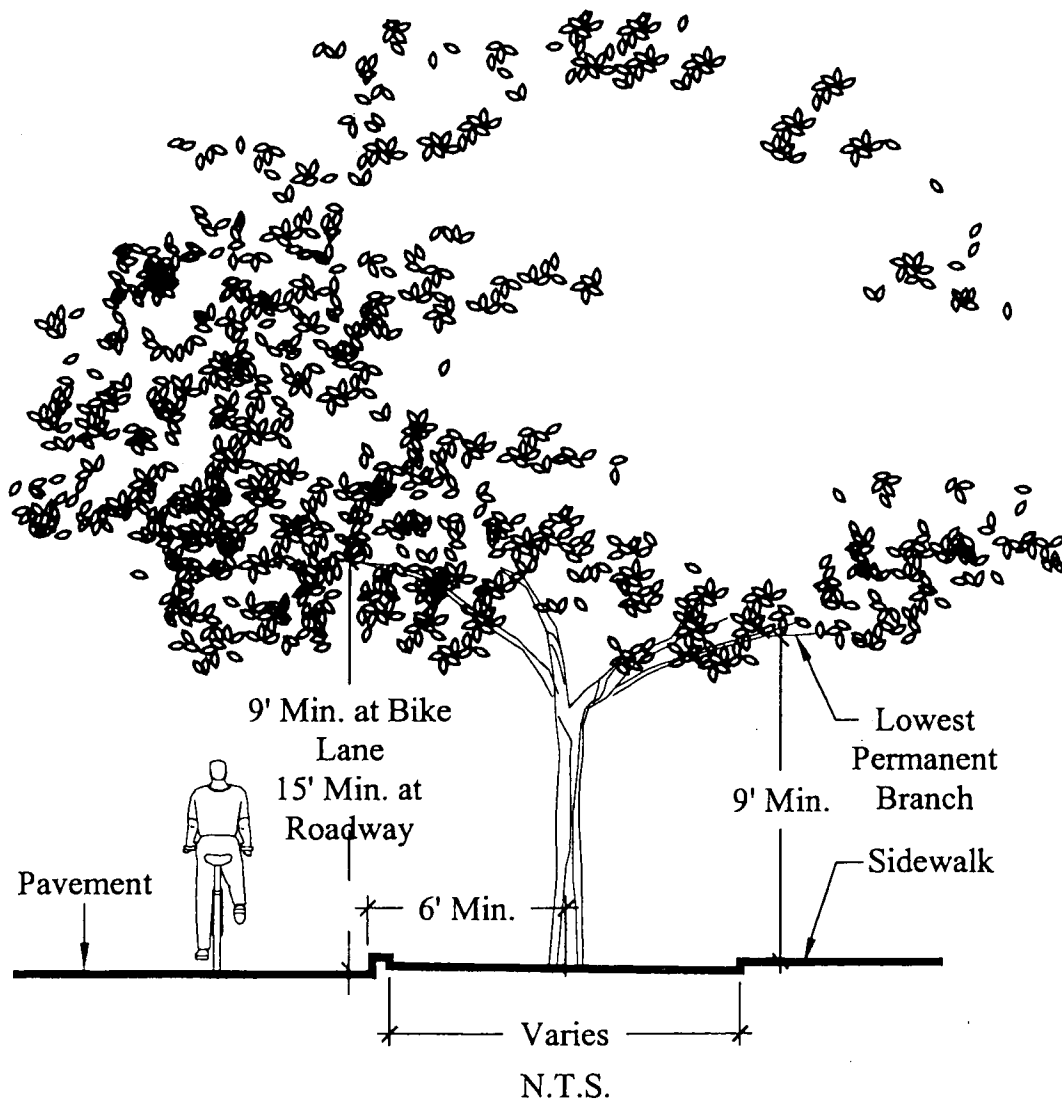
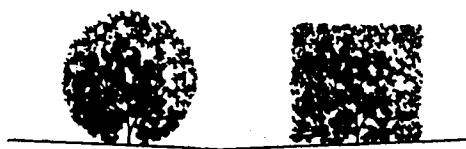


Figure 12
Shrubs and Groundcover Pruning



NO!

Costly over pruning once per month

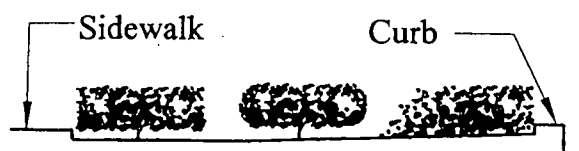
- preventing flowering
- promoting woody growth
- requiring more water after frequent pruning



YES!

Pruning of shrubs to maintain a full, natural shape & flowering once per year

Sq. Cut-off pruning at edges



NO!

Each individual groundcover is pruned separately which is time consuming & wasteful. Lack of trimming top of groundcovers makes plants appear as small shrubs. Square pruning at edges at sidewalk / curbs forces the necessity for frequent trimming to keep plants off of sidewalk / curbs

Trimmed at an angle along edges



YES!

Less trimming by allowing plants to grow together only top trimming and angle trimming at edges required

5.3 Walkways, Crosswalks and Median Ends

Walkways within the Gateway Area consist of sidewalks, crosswalks and access ways. All construction shall be per Town of Gilbert standards.

Sidewalks shall be six (6) feet, eight (8) feet or fifteen (15) feet in width, along both sides of each arterial street. The eight-foot (8) wide walkway occurs on Williams Field Road (Section 2.3) and the fifteen-foot (15) wide walkway occurs in the Village Center (Section 3.3).

Sidewalks shall be concrete, and constructed to Town of Gilbert standards.

Crosswalks shall be provided as required by the Town. Crosswalks shall be constructed of interlocking concrete pavers, set in a 90-degree herringbone pattern or as approved by the Town Engineer. Pavers shall be Cambridge Cobble, 6"x9" in size, 80 mm (8 cm) thick, color shall be Fuego Blend, or approved equal. Crosswalks shall be twelve (12) feet wide, with an additional ten-inch (10") wide concrete restraint curb on each side of the crosswalk, delineated as required by adopted Town standards. There shall be a nine-inch (9) wide trim row inside the restraint curb. Pavers shall meet ADA requirements. Figure 13 shows cross section and plan details.

Medians shall contain interlocking concrete pavers, set in a 90-degree herringbone pattern or as approved by the Town Engineer, and shall extend from the median bullnose until the inside width equals or exceeds four (4) feet. The pavers shall be Cambridge Cobble, 6"x9" in size, 80 mm (8 cm) thick, color shall be Fuego Blend, or approved equal. A six-inch (6") wide concrete header shall be installed at the end of the pavers to be flush with the median curb. Figure 14 shows cross section and plan details.

Access ways are required through the landscape buffer easements at appropriate locations to facilitate pedestrian and bicycle connection to arterial walkways and transit stops. These are to be six (6) feet in width and constructed of concrete, per Town of Gilbert Standard Detail 42.

Figure 13
Crosswalk Plan and Section

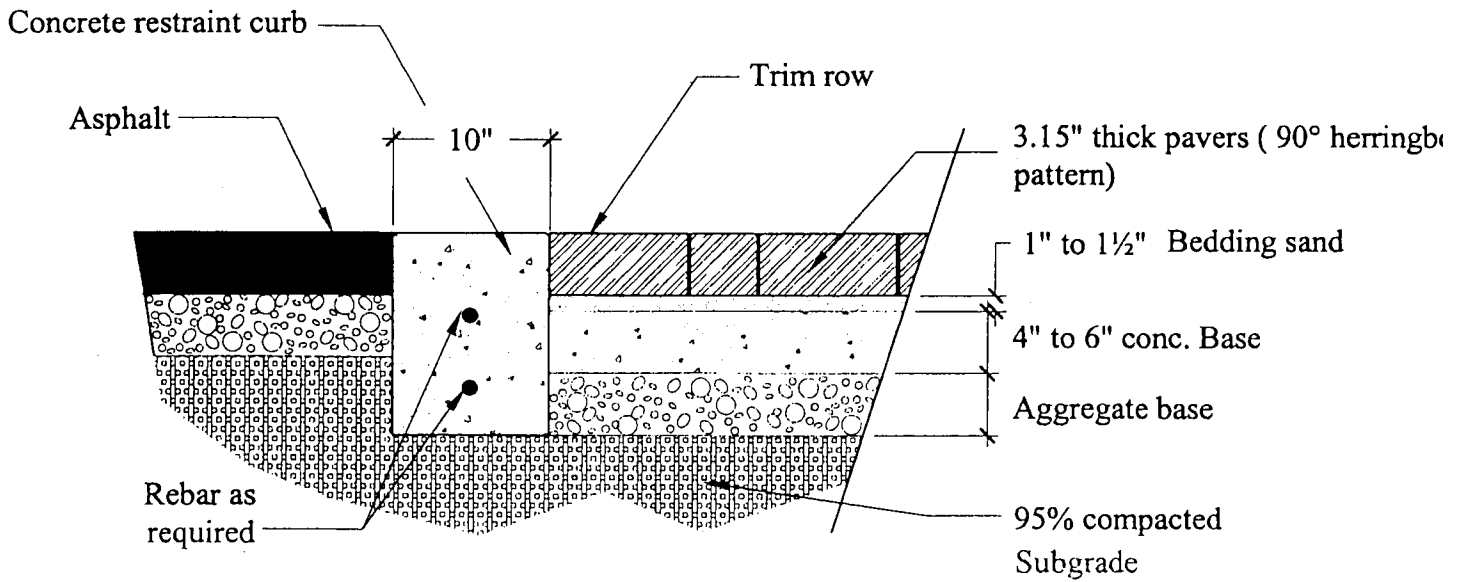
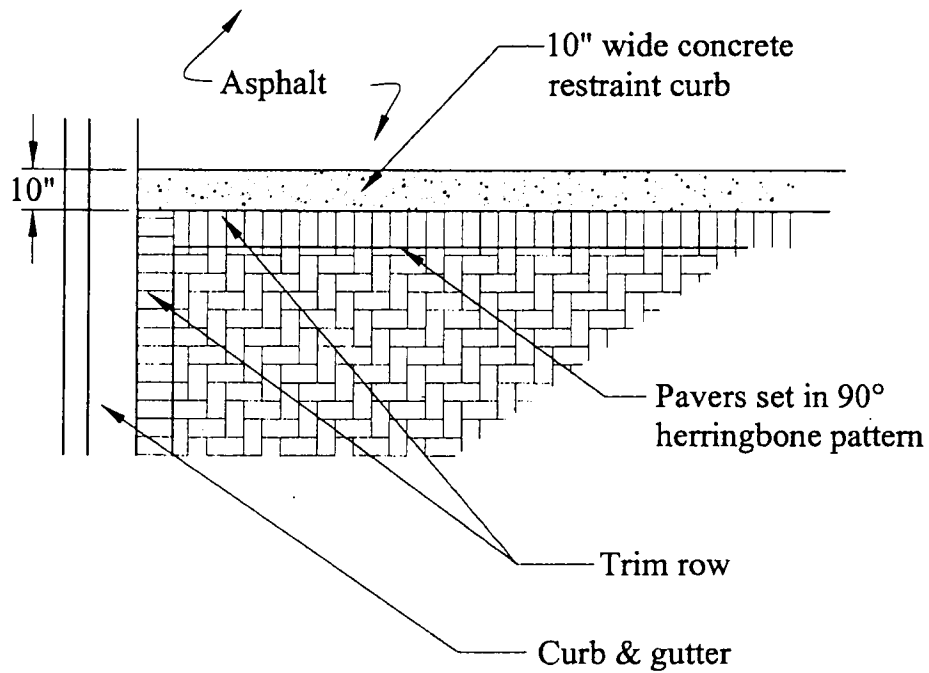
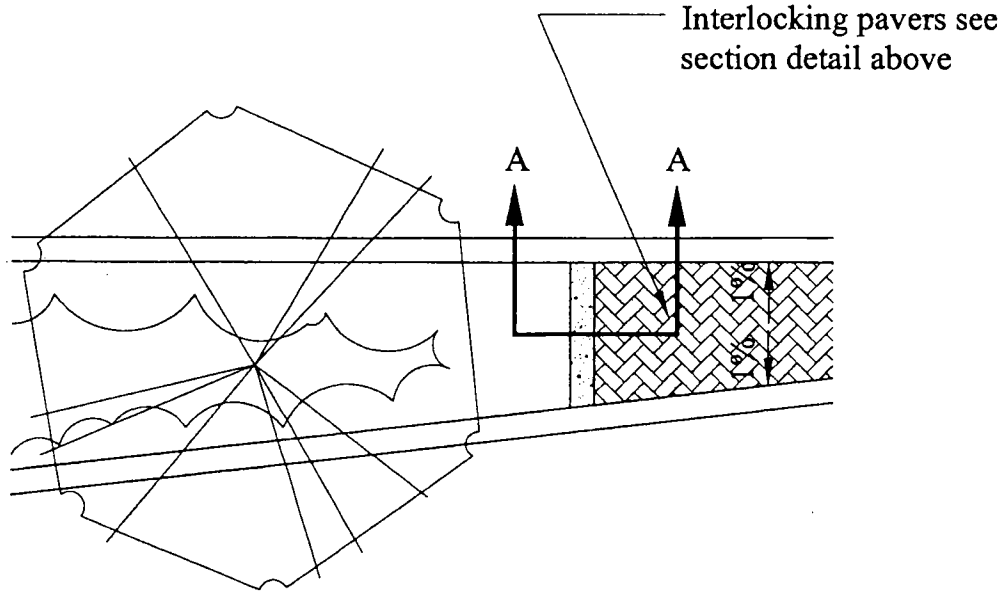
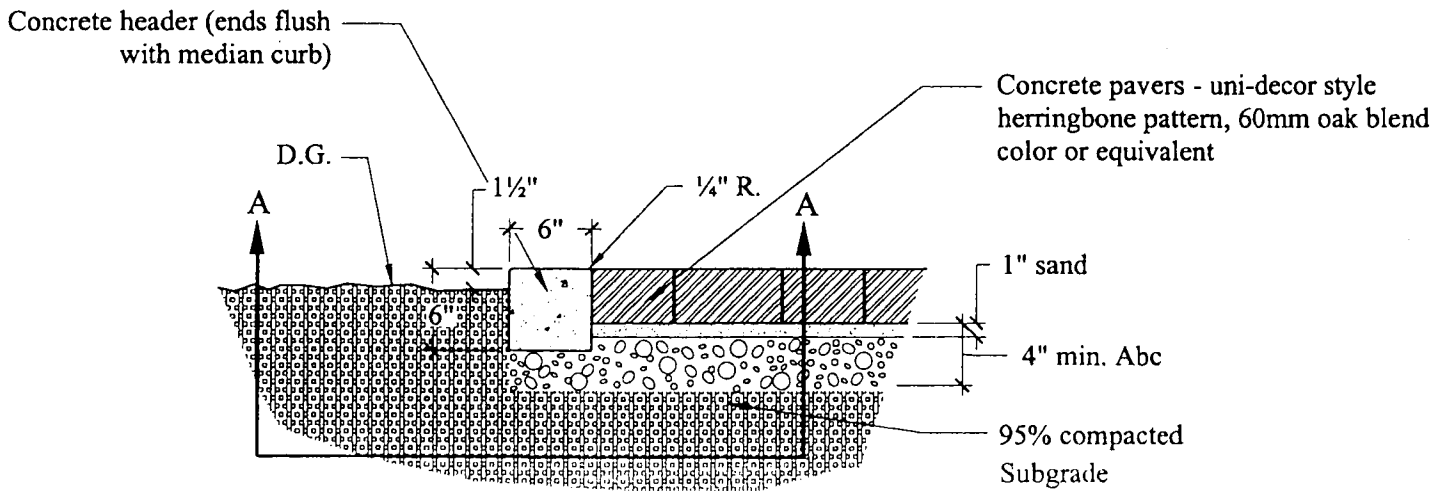


Figure 14
Median End Plan and Section

Plan View



Section



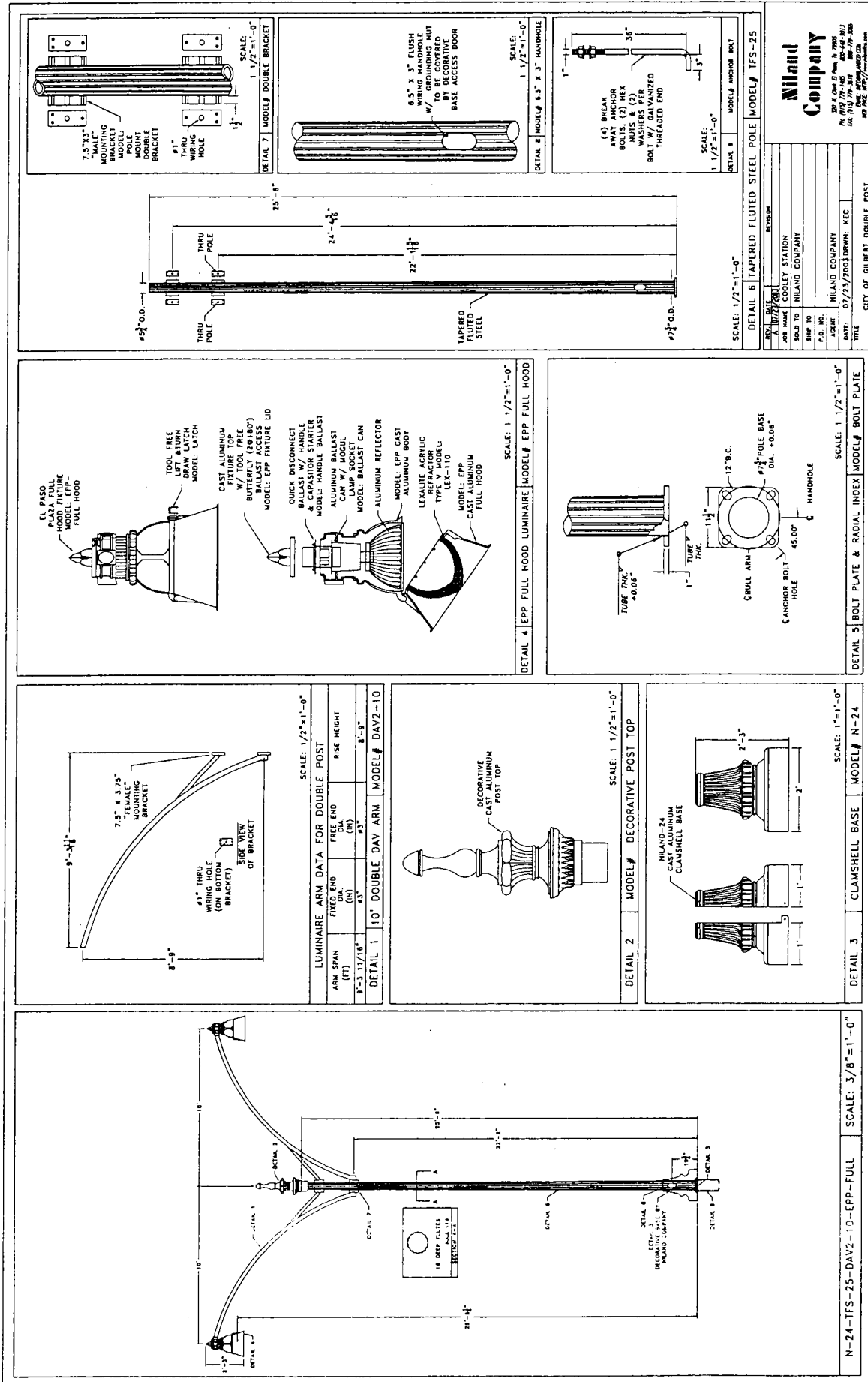
5.4 Lighting

Lighting is a key character element in the Gateway Area and consists of two primary types: street lights; and landscape (pedestrian) area lights.

Street lights shall be Niland Town of Gilbert Series, "Brown Bronze" in color, or as approved by the Public Works Director, based on technical data meeting sound engineering standards. Median lights in arterial roads shall be double arm, with spacing of 200 feet on center, or as required by the Traffic Engineer. Street lights located along collector streets, shall be single-arm, Niland Town of Gilbert Series, "Brown Bronze" in color, or as approved by the Public Works Director. Collector street lights shall be spaced as required by the Traffic Engineer. Street lights along arterials shall have a pole height of twenty-five feet eight inches (25' 8") with a corresponding height of twenty-nine feet six and one-fourth inches (29' 6 ¼") to the luminaire or as approved by the Public Works Director, based on technical data meeting sound engineering standards. Street lights along collectors shall meet the same height requirements to top of pole and height to luminaire as street lights or as approved by the Public Works Director. Figures 15 and 16 contain the specifications for these two types of street lights. As possible, street names and traffic control signs shall be placed on street light poles, as illustrated in Figure 17. Street lights within public rights-of-way shall be maintained by the Town of Gilbert. Light poles shall be break-away type, and specifications shall be as approved by the Public Works Director. These lights are not required on local streets, but may be used if approved by the Town. Street lights within planting or non-paved areas shall be placed on a concrete base that is twenty-four inches (24") in diameter and twelve inches (12") above finished grade.

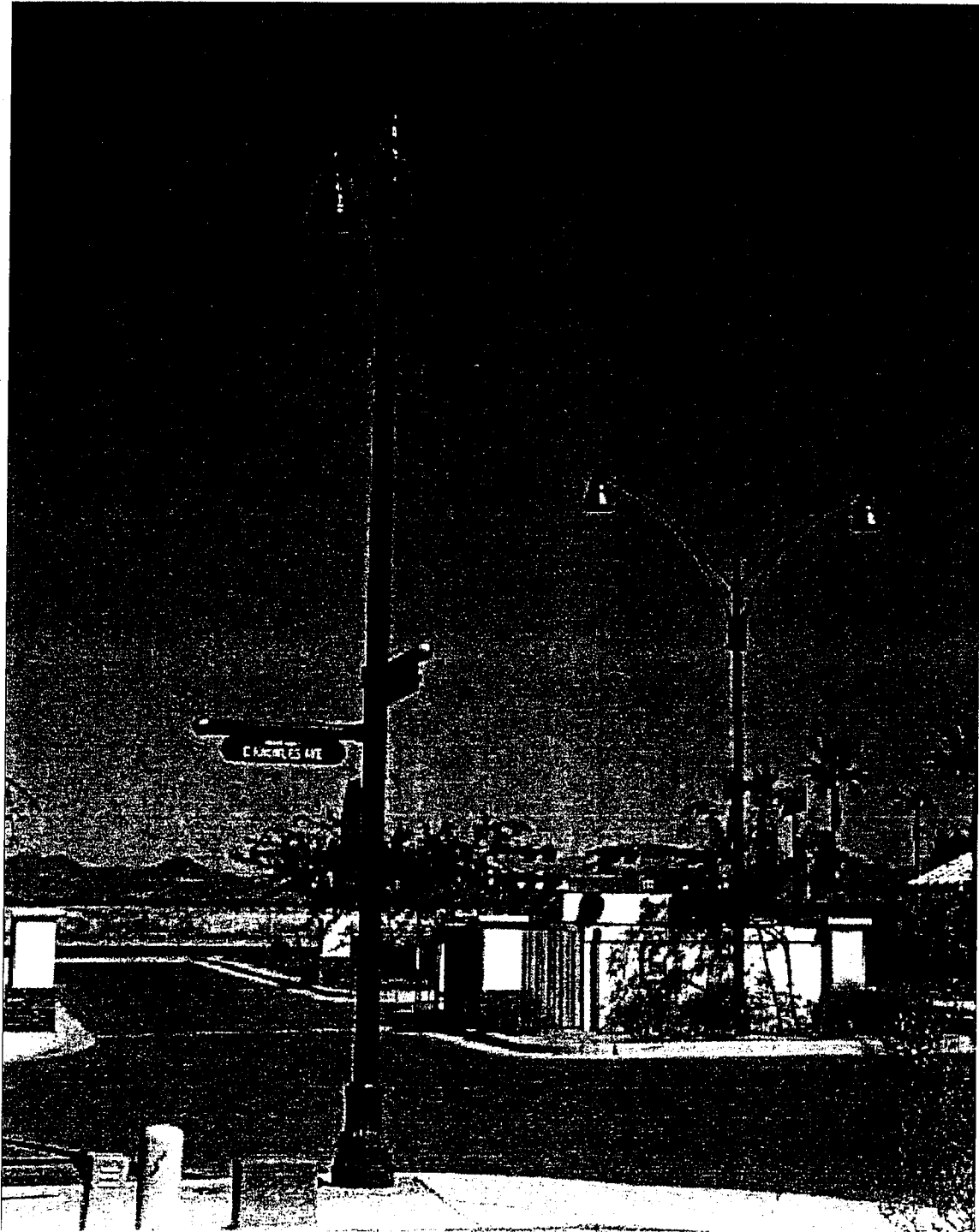
Landscape (pedestrian) area lights shall be Niland Town of Gilbert Double Post Capitol Series, "Brown Bronze" in color, or approved equal. Lights are to be located within landscape areas between the curb and walkway, as shown on the appropriate road cross section and layout plan in Section 2.0. These lights are required for arterial streets, but are optional adjacent to collector and local streets. Lights shall be one hundred thirty (130) feet on center. Landscape (pedestrian) area lights shall be twelve (12) feet in height. Figure 18 illustrates the landscape (pedestrian) area light. Landscape (pedestrian) area lights shall be maintained by the adjacent homeowners' or property owners' association.

Double Arm Street Light



[illegible]

Figure 17
Double and Single Arm Street Lights with Traffic Control Signs



DETAIL 1 MODEL # CON-1C DECORATIVE TOPPER
SCALE: 4"=1'-0"

DECORATIVE CAST ALUMINUM CON-1C LUMINAIRE TOPPER
16 1/8"

DETAIL 2 MODEL # CAPITOL POST TOP
SCALE: 4"=1'-0"

PHOTO EYE
16 1/8"

DETAIL 3 MODEL # N-18 BASE
SCALE: 1 1/2"=1'-0"

TRANSITION RING TO MIRROR FLUTED DESIGN
1'-7"

DETAIL 4 5" FLUTED POLE MODEL # F5
SCALE: 1"=1'-0"

10'

DETAIL 5 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 6 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

12'-10 3/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 7

DECORATIVE CAST ALUMINUM BASE BY NILAND

DETAIL 8 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 9 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 10 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 11 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 12 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 13 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 14 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 15 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 16 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 17 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 18 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 19 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 20 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 21 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 22 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 23 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 24 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 25 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 26 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 27 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 28 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 29 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 30 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 31 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 32 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 33 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 34 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 35 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 36 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 37 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 38 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 39 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 40 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 41 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 42 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 43 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 44 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 45 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 46 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 47 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDHOLE

DETAIL 48 MODEL # CP-CON-1C PHOTO
SCALE: 2"=1'-0"

16 1/8"

2'-10 1/8"

8'-5"

1'-7"

PHOTO EYE

DETAIL 49 MODEL # ANCHOR BOLT
SCALE: 4"=1'-0"

18"

3"

(1) BREAK AWAY ANCHOR BOLTS, (2) HEX NUTS & (2) WASHERS PER BOLT
BOLT GALVANIZED THREADED END

DETAIL 50 MODEL # BOLT PLATE
SCALE: 1"=1'-0"

18"

1'-7"

Ø 12" B.C.
F HANDH

5.5 Traffic Signals and Signs

Traffic signals and traffic signs shall be standard Town of Gilbert fixtures. Traffic signals and other traffic control devices shall be reviewed during the design process for compliance with local and national standards. Where possible, street name signs and traffic control signs shall be hung on street light poles. See Figure 17.

5.6 Bus/Pedestrian Shelters

A standard, RPTA mid-block bus bay shall be provided at the far side of each arterial street intersection where there are existing or proposed bus routes. Additional bus bays may be provided on the far side of half-mile, signalized intersections as approved by the RPTA. Each bus bay shall include a bus shelter, bench, trash receptacle, bus shelter pad and pedestrian lighting. Light fixtures shall be the same series as landscape (pedestrian) area lighting detailed in Section 5.4. Interlocking concrete pavers shall be utilized in the bus shelter pad area. Pavers are to be the same as those in crosswalks, as detailed in Section 5.3, or approved equal. Pavers shall meet ADA requirements. A standardized design for the bus shelter, bench and trash receptacle shall be approved by the Town of Gilbert Design Review Board. Figures 19 and 20 contain standard details for the mid-block bus bay and bus shelter pad.

5.7 Utility and Driveway Locations

Existing and future electrical and utility lines shall be placed underground within street rights-of-way or in adjacent utility easements. This requirement does not apply to electric utilities that are 69kV or larger. Utility locations shall be per Town of Gilbert Standard Details 21A and 22A except as modified by the Town Engineer. Driveway cuts and utility cabinet locations will be sited to minimize impact to street tree theme. Traffic signal controllers shall be sited in such a way as they have visual contact with all legs of an intersection and shall be located near the sidewalk.

The Roosevelt Water Conservation District (RWCD) will have facilities along portions of Higley Road, Warner Road and Ray Road to transport irrigation water within their delivery area. These irrigation pipes will be located underneath the six-foot wide sidewalk (see Figure 2) as approved by the Town Engineer. Trees must be set back a minimum of five (5) feet from the centerline of the pipe. Associated delivery structures will be located in the twelve-foot (12) wide landscape area between the sidewalk and curb.

Figure 19
Mid-block Bus Bay

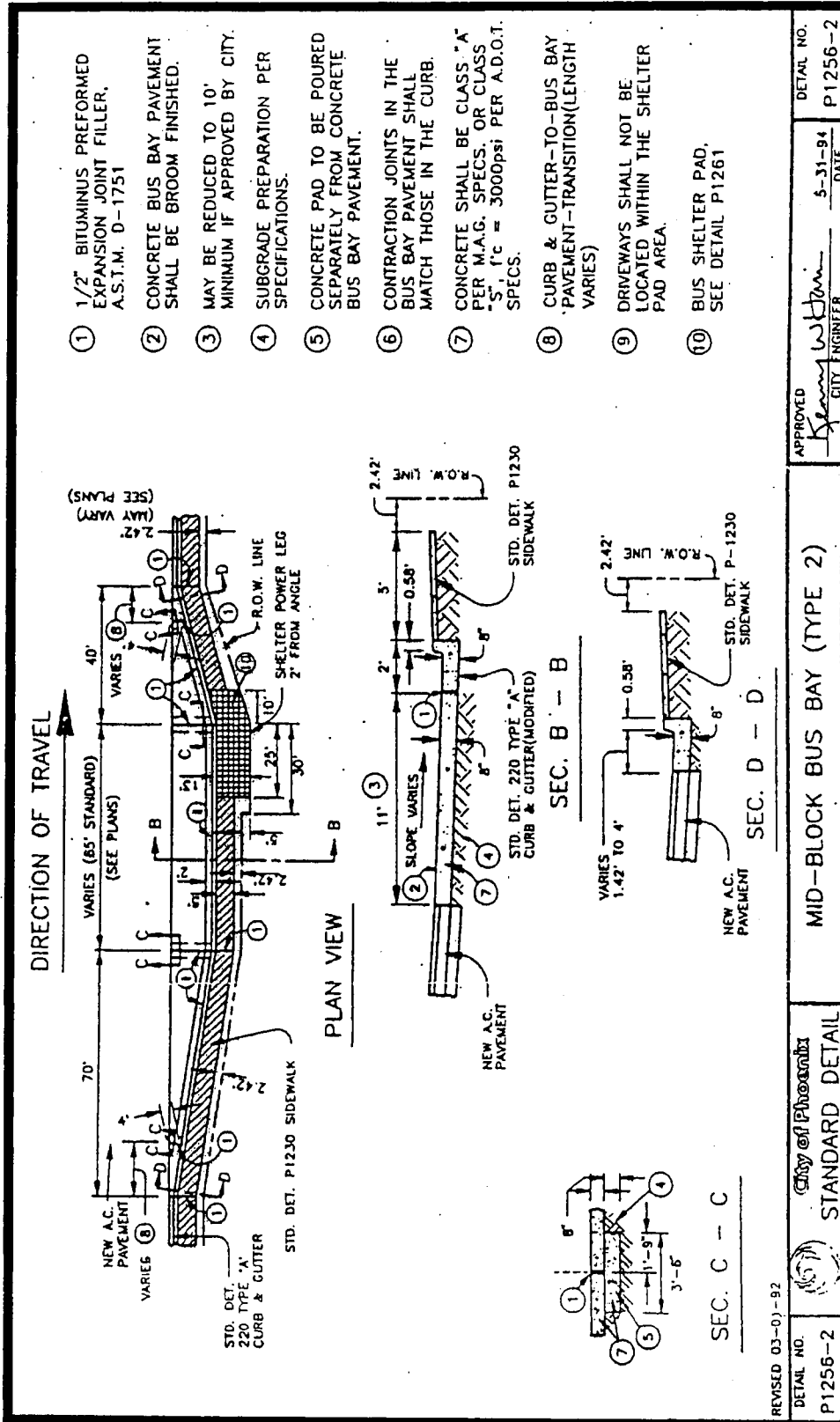
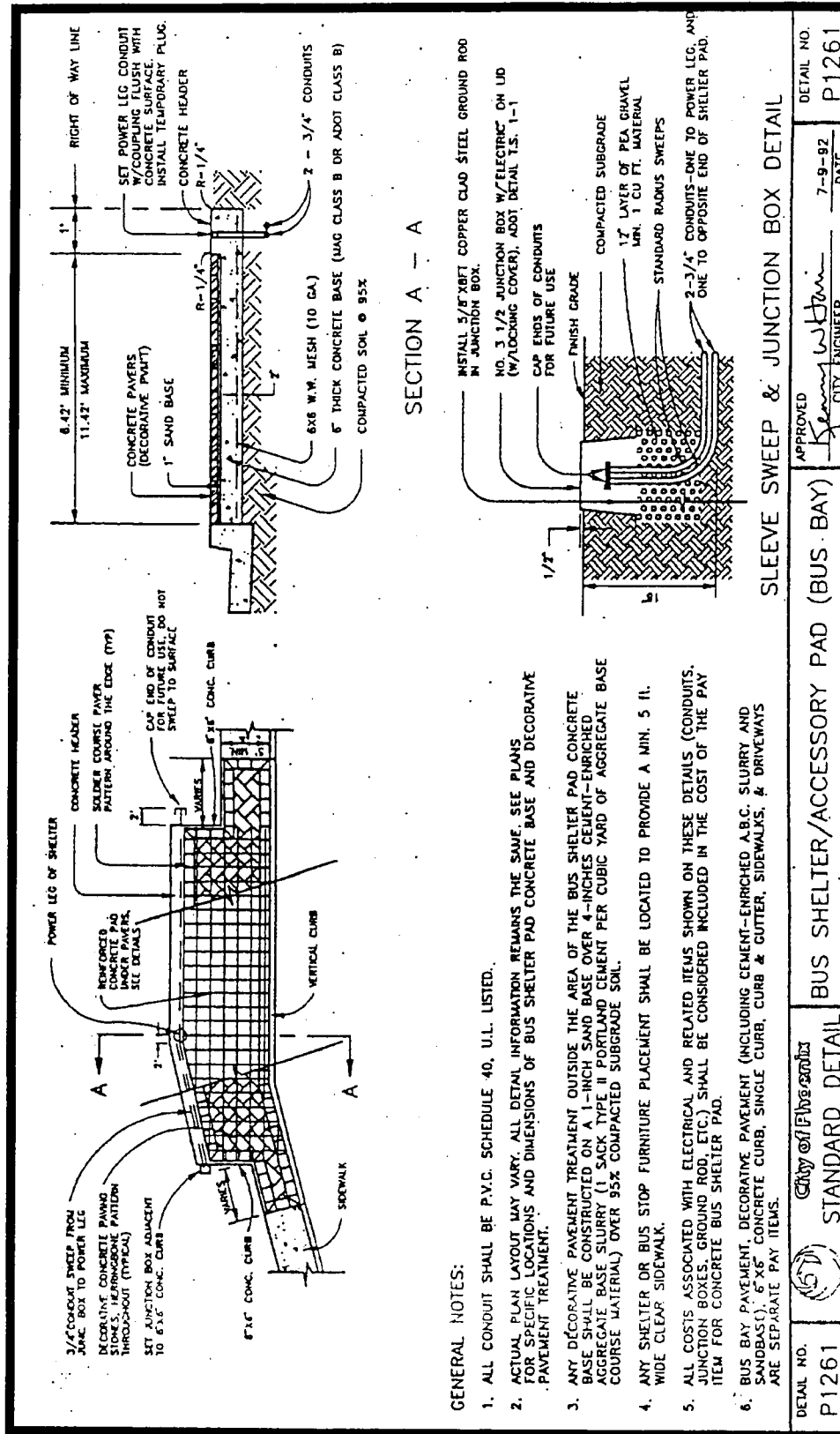
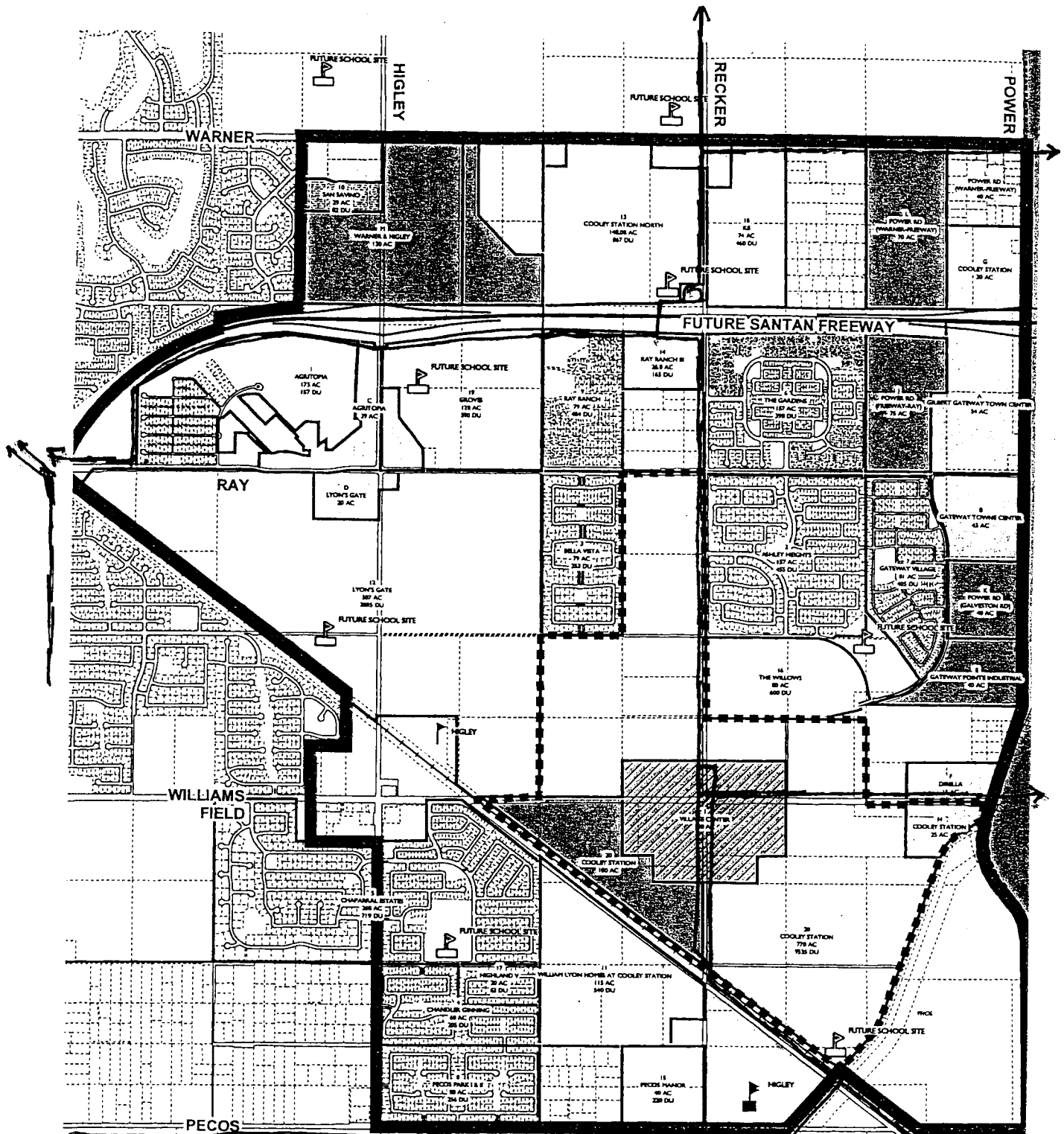


Figure 20
Bus Shelter/Accessory Pad





GATEWAY AREA PROJECTS - RESIDENTIAL			GATEWAY AREA PROJECTS - COMMERCIAL		
Subarea/Project Name	Size	Units	Subarea/Project Name	Size	Units
1. Agave	175 AC	187 DU	A. Gateway Town Center (Phase 1, N. of Ray)	84 AC	(195,000 sq ft)
2. Agave Heights	177 AC	488 DU			
3. Agave Hills	79 AC	283 DU			
4. Chandler Crossing	68 AC	388 DU			
5. Chandler Crossing	368 AC	719 DU			
6. The Gardens	187 AC	349 DU			
7. Gateway Village	81 AC	488 DU			
8. Gateway Village I & II	88 AC	334 DU			
9. Ray Ranch	75 AC	489 DU			
10. Ray Ranch (Future)	28 AC	43 DU			
	1164 AC	3414 DU			
Assessments					
11. Willow Grove in C.R.	115 AC	549 DU			
12. Lyon's Gate	818 AC	3888 DU			
13. Cooley Station North	148 AC	807 DU			
14. Ray Ranch II	57 AC	148 DU			
15. Pecos Ranch	89 AC	128 DU			
16. Williams	69 AC	488 DU			
17. Highland V	38 AC	43 DU			
	985 AC	5440 DU			
Assessments, Limited, Various					
18. S.E. Pecos	74 AC	488 DU			
19. Pecos	138 AC	390 DU			
	212 AC	1078 DU			
Assessments, Limited, Various					
20. Cooley Station	778 AC	1938 DU			
	3117 AC	19489 DU			

Legend

- Gateway Area Land Use
 - Residential
 - Commercial
 - Industrial/Business Park
 - Village Center
- Other
 - Phon
 - Queen Creek
 - Gilbert Planning Boundary
- Gateway Area Boundary
- Cooley Station Boundary

Scale: 0 375 750 1,500 Feet
1 inch equals 1,500 feet
April 13, 2004

GATEWAY AREA MAP

GILBERT